The background of the cover is a photograph of a boat ramp, overlaid with a green-to-blue gradient. The ramp is made of concrete and leads down to a body of water. Several white boat lifts are visible along the sides of the ramp. The text is overlaid on the upper portion of the image.

Environmental Assessment for Dredging at USCG Base Elizabeth City MWR Boat Ramp Boat Basin and Moorings, Elizabeth City, North Carolina

USCG Base Elizabeth City
Dredging at MWR Boat Ramp
Elizabeth City, NC
USCG Project No. 3756419
TO HSCG83-14-J-PCR197

U.S. COAST GUARD

FINDING OF NO SIGNIFICANT IMPACT

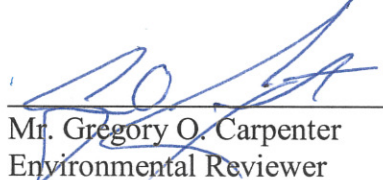
FOR

**BOAT BASIN AND MOORING DREDGING AT USCG BASE ELIZABETH CITY
MWR BOAT RAMP, ELIZABETH CITY, NORTH CAROLINA**

This project has been thoroughly reviewed by the USCG and it has been determined, by the undersigned, that this project will have no significant effect on the human and natural environment.

This Finding of No Significant Impact (FONSI) is based on the attached contractor prepared Environmental Assessment (EA) which has been independently evaluated by the USCG and determined to adequately and accurately discuss the environmental issues and impacts of the proposed project and provides sufficient evidence and analysis for determining that an environmental impact statement is not required. The USCG takes full responsibility for the accuracy, scope, and content of the attached environmental assessment.

9/14/15
Date


Mr. Gregory O. Carpenter
Environmental Reviewer

Chief, Environmental Compliance
Title/Position

I have considered the information contained in the EA, which is the basis for this FONSI. Based on the information in the EA and this FONSI document, I agree that the proposed action as described above, and in the EA, will have no significant impact on the environment.

9/15/2015
Date


Responsible Official

Commanding Officer
USCG CEU Cleveland
Title/Position

**U.S. COAST GUARD
ENVIRONMENTAL ASSESSMENT
FOR**

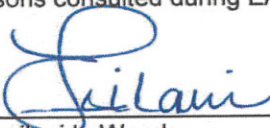
USCG BASE ELIZABETH CITY MWR BOAT RAMP BOAT BASIN AND MOORING IMPROVEMENTS

This USCG Environmental Assessment was prepared in accordance with Commandant's Manual Instruction M16475.1D and is in compliance with the National Environmental Policy Act of 1969 (P.L. 91-190) and the Council of Environmental Quality Regulations dated 28 November 1978 (40 CFR Parts 1500-1508).

This Environmental Assessment (EA) serves as a concise public document to briefly provide sufficient evidence and analysis for determining the need to prepare an Environmental Impact Statement (EIS) or a Finding of No Significant Impact (FONSI).

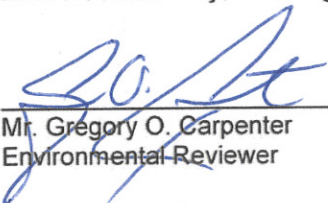
This EA concisely describes the proposed action, the need for the proposal, the alternatives, and the environmental impacts of the proposal and alternatives. This EA also contains a comparative analysis of the action and alternatives, a statement of the environmental significance of the preferred alternative, and a list of the agencies and persons consulted during EA preparation.

10 Sept 2015
Date


Ms. Leilani L. Woods
Environmental Project Manager

Environmental Engineer
Title/Position

9/14/15
Date


Mr. Gregory O. Carpenter
Environmental Reviewer

Chief, Environmental Compliance
Title/Position

In reaching my decision/recommendation on the USCG's proposed action, I have considered the information contained in this EA on the potential for environmental impacts.

9/15/2015
Date


Responsible Official

Commanding Officer
USCG CEU Cleveland
Title/Position

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Appendix F.	Permits

List of Acronyms

AECOM	AECOM Technical Services, Inc.
APE	Areas of Potential Effect
BMP	Best Management Practice
CEQ	Council on Environmental Quality
CZMP	Coastal Zone Management Program
CZMA	Coastal Zone Management Act
EA	Environmental Assessment
FONSI	Finding of No Significant Impact
LQG	Large Quantity Generator
MWL	Mean Water Level
MWR	Morale, Welfare and Recreation
NAAQS	National Ambient Air Quality Standards
NAVD88	North American Vertical Datum 1988
NC DCM	North Carolina Division of Coastal Management
NCDENR	North Carolina Department of Environment and Natural Resources
NC DWR	North Carolina Division of Water Resources
NC NHP	North Carolina Natural Heritage Program
NC WRC	North Carolina Wildlife Resources Commission
NEPA	National Environmental Policy Act
NMFS	National Marine Fisheries Service
PNC	Pre-Construction Notification
RB-S	Response Boat-Small
SHPO	State Historic Preservation Office
SPC	Special Purpose Craft
TES	Threatened and Endangered Species
USACE	United States Army Corps of Engineers
US EPA	United States Environmental Protection Agency
USCG	United States Coast Guard
USFWS	United States Fish and Wildlife Service

1 Project Justification

1.1 Introduction

AECOM Technical Services, Inc. (AECOM) completed this Environmental Assessment (EA) on behalf of the United States Coast Guard (USCG). The EA evaluates the potential environmental impacts associated with the proposed dredging of the boat basin and moorings at the Morale, Welfare, and Recreation (MWR) Boat Ramp at the USCG Base Elizabeth City in Pasquotank County, North Carolina. The EA was performed in accordance with: National Environmental Policy Act (NEPA) of 1969 (Section 102[2][c]), as implemented by the Council on Environmental Quality (CEQ) regulations (40 *Code of Federal Regulations* parts 1500-1508); *Procedures for Considering Environmental Impacts* (Department of Transportation 1979, rev. 1982, rev. 1985) Order 5610.1C; and USCG Policy *NEPA: Implementing Procedures and Policy for Considering Environmental Impacts*, COMDTINST M16475.1D (USCG, 2000). NEPA requires the assessment of environmental consequences of Federal actions that may affect the quality of the human and natural environment. Based on the potential for impacts described herein, the USCG will either publish a Finding of No Significant Impact (FONSI) or prepare an Environmental Impact Statement.

The USCG Base Elizabeth City is located approximately 3.5 miles southeast of Elizabeth City, along Weeksville Road / Highway 344 (formerly Highway 34), on the south shore of the Pasquotank River in Pasquotank County, North Carolina (**Figure 1**). The geographic location of the MWR Boat Ramp is latitude 36° 16' 14.72" north and longitude 76° 10' 40.31" west. The MWR Boat Ramp can be approached by water from the northeast or by land via Moukawsher Drive along the shoreline. Moukawsher Drive can be accessed from Consolidated Road on the south end of the Base.

The MWR Boat Ramp is located on the north end of the Base's shoreline, in a portion of the river tidal flats. The MWR Boat Ramp consists of a two-lane concrete ramp with a concrete pavement approach and a launch dock centered in the boat ramp. Rubble mound stone breakwaters project approximately 100 to 125 feet from the shoreline on both sides of the launch ramp to form a protected harbor area for launching and retrieving boats. Timber docks line both sides of the boat basin. Mooring piles are in place along the west timber dock. The MWR Boat Ramp is utilized by Base military personnel for launching privately-owned recreational boats, and for launching USCG vessels stationed at the Building 43 moorings. **Figure 2** is an aerial photograph of the boat ramp, boat basin, and moorings. Photographs of the MWR Boat Ramp are included in **Appendix A**.

1.2 Purpose and Need Statement

The purpose of this project is to dredge the boat basin and moorings to a required depth below mean water level (MWL) at the MWR Boat Ramp due to changing conditions at the site. Water depth in the boat basin and moorings is reduced by natural sediment deposition and by heavy winds from the north, which push water from the boat basin and down the Pasquotank River. This wind effect decreases the depth of the water and hinders the USCG's ability to utilize the boat ramp for both operational missions and recreational purposes. According to the USCG personnel at Base Elizabeth City, the sustained winds can result in lowering water levels as much as 2 to 3 feet. Recent sounding data measured at the MWR Boat Ramp indicates that depth to sediment is as shallow as 4 feet MWL while the draft, the vertical distance between the waterline and the bottom of the hull, for the Response Boat-Small (RB-S) vessels is 3 feet 3 inches. Together, these conditions create inadequate clearance depth for launching Special Purpose Craft (SPC) and RB-S vessels. As a result, the USCG is routinely forced to launch their vessels at a public boat launch a number of miles upriver, which results in up to an hour delay for mission response. The USCG proposes dredging the MWR Boat Ramp to a maximum depth of 8 feet below MWL. MWL corresponds to elevation -0.44 North American Vertical Datum 1988 (NAVD88). Therefore, dredging will proceed to -8.44 feet NAVD88.

1.3 Alternatives Considered

In order to provide the sufficient mooring depth for the MWR Boat Ramp, the following alternatives were considered and either dismissed or evaluated in this EA.

1.3.1 Alternatives Considered and Dismissed

Alternatives that would not allow the USCG Base Elizabeth City to safely use the boat launch and moorings were considered and dismissed because the operational missions of the USCG Base Elizabeth City could not be safely and efficiently executed. Alternatives considered and dismissed included: 1) Acquiring alternative vessels that would require less clearance depth for launching was considered and dismissed because the USCG has no plans for such changes; 2) Discontinuing use of the MWR boat ramp as a boat launch for their search and rescue vessels and having to launch their vessels at a public boat launch several miles upriver was considered and dismissed because this results in up to an hour delay for mission response; 3) No longer allowing military personnel to use the boat ramp for recreational purposes was considered and dismissed because the boat ramp is used in conjunction with on-site cabins, mobile home units, recreational vehicle stations, and various other recreational facilities available to USCG personnel and their families; and, 4) Constructing a new boat ramp along the Base's shoreline was considered and dismissed because this is not a feasible option due to the financial investment and that a new boat ramp would still need to be constructed along the shoreline of the Pasquotank River for the USCG's use. This would result in new dredge work in an undisturbed area, more significant and longer duration of impacts, and inevitably maintenance dredging will be required in the future. Therefore, this is not a practical long-term solution.

1.3.2 Alternatives Considered for Evaluation

No Action: This alternative would have the USCG continuing to use the boat basin and moorings in its current condition.

Alternative A: Dredge to a Maximum Depth of 8 Feet: This alternative proposes mechanical dredging of accumulated sediment at the boat basin and moorings area to a target depth of 8 feet below MWL. The MWR Boat Ramp is a previously dug man-made basin and the proposed dredging will occur entirely within the previously disturbed footprint of the boat basin and moorings. However, the USCG has no record of the as-built drawings indicating the original dredge depth. While the dredging required for the original construction of the MWR Boat Ramp likely disturbed the sediments to a depth proximate to the proposed dredge depth, the exact depth of the original dredging is not documented. The dredging of the boat basin and moorings will remove sediment accumulated since the MWR Boat Ramp was initially constructed. MWL corresponds to elevation -0.44 NAVD88; therefore, dredging will proceed to -8.44 feet NAVD88.

Approximately 755 cubic yards of sediment will be dredged from the boat basin and moorings. During the proposed dredging activities, the dredge area will be closed with an impermeable floating turbidity curtain with a weighted bottom edge. The dredge material will be deposited on barges, dewatered from the barge within the turbidity curtain, transported by barge to an unloading site, offloaded to trucks, and transported by truck to the licensed East Carolina Regional Landfill in Aulander, North Carolina for disposal. Formal approval for disposal of the dredge material at the MWR Boat Ramp has been received from the East Carolina Regional Landfill. Design drawings for the proposed alternative are in **Appendix B**.

The proposed dredging project will only temporarily impact open waters (an area totaling approximately 0.18 acres) and will occur entirely within the existing boat basin limits (limited to an area approximately 150 feet from the shoreline).

1.4 Area of Potential Effect

The area of potential effect (APE) includes both on-shore and off-shore portions of the MWR Boat Ramp (see **Figure 2**). The APE also includes access roads to the MWR Boat Ramp and land transportation routes to the landfill. The landfill where the sediment will be disposed of was included in the APE for evaluation of cultural

resources. The landfill was not included in the evaluation of other impacts since the disposal of sediment at the landfill is consistent with the current and licensed use of the landfill.

2 Summary of Environmental Impacts

This section evaluates the significance of environmental impacts of the proposed project on the physical, natural, socioeconomic, and cultural environment. CEQ regulations (40 *Code of Federal Regulations* 1508.27) define “significance” in terms of the *context* of the action and the *intensity* of the impacts. The context considers society as a whole, the affected region, the affected interests, and the locality. The intensity of impact refers to the severity of an impact, and the following factors are considered:

- Beneficial and adverse impacts
- Public health and safety
- Unique geological characteristics
- Controversial nature of the action
- Uncertain effects
- Precedent-setting actions
- Cumulative impacts
- Historic landmark impacts
- Impacts to endangered or threatened species or their habitat
- Potential for violation of Federal, state, or local environmental standards

The duration of the impacts are also considered. Temporary impacts are reduced early in the project, short-term impacts occur during the life of the project and long-term impacts exist after project completion.

2.1 Physical Environment

2.1.1 Geology, Topography, and Soils

Affected Environment: The USCG Base Elizabeth City is situated on the south shore of the Pasquotank River, which connects to the Albemarle Sound, and is part of the Intercostal Waterway. The MWR Boat Ramp is located on the north end of the base’s shoreline, in a portion of the river tidal flats. Grain size analysis conducted on three sediment samples indicated that the sediment material consists of 4.4 to 6.8 percent fines (passing a # 200 sieve). Pasquotank County is located within the North Carolina Coastal Plain, and the geology of the area consists of undivided surficial deposits of sand, clay, and gravel of the Quaternary period. According to the Natural Resources Conservation Service Soil Survey maps, the area immediately adjacent to the dredge area consists of Udorthents loamy soils on zero to two percent slopes.

Anticipated Impacts: The ‘no action’ alternative will have no impacts on the physical environment. Proposed dredging of the MWR Boat Ramp will remove sediment from the boat basin and moorings to depths of up to 8 feet below MWL. Potential impacts associated with soil erosion are not significant because land areas will not be disturbed outside of already developed areas, and dredged sediments will be deposited onto a barge prior to transport to an unloading site.

Mitigations and Conclusions: The dredging of the area affects only sediment accumulated within the boat basin and mooring area due to natural deposition and lower water levels. The dredging will not have significant impacts to the local geology and soils because of the small area and volume of sediment. No mitigation is necessary.

2.1.2 Climate and Air Quality

Affected Environment: Average temperatures in Elizabeth City range from a minimum of 31 degrees Fahrenheit in January to 89 degrees Fahrenheit in July. The mean annual temperature is 61 degrees. The area receives approximately 49 inches of precipitation a year. According to the information from the State Climate Office of North Carolina, the average wind speed at the Coast Guard Air Field in Elizabeth City for 2014 was 8.9 miles per hour. Pasquotank County has good air quality, with air quality index values below 50 since 1999. An air quality index of 50 or below represents good air quality with little potential to affect public health.

The United States Environmental Protection Agency (US EPA), under the requirements of the 1970 Clean Air Act as amended in 1977 and 1990, established primary and secondary standards for six airborne pollutants or criteria pollutants: carbon monoxide, nitrogen dioxide, ozone, particulate matter, lead, and sulfur dioxide. The primary standards, known as National Ambient Air Quality Standards (NAAQS), are intended to protect public health. The secondary standards are intended to protect public welfare and account for air pollutant effects on soil, water, visibility, material, vegetation, and other aspects of general welfare. The North Carolina ambient air quality standards include all of the NAAQS plus a standard for total suspended particulate matter and particulate matter with a diameter of ten microns or less. For each pollutant, NAAQS has two designations: attainment areas that meet the NAAQS and non-attainment areas that do not meet the NAAQS. Currently, Pasquotank County is considered in attainment for the NAAQS and North Carolina Department of Environment and Natural Resources (NCDENR) Standards.

Anticipated Impacts: The 'no action' alternative will have no impacts on climate and air quality. Emissions caused by dredging and transporting the sediment by truck are too minor to have a significant effect on climate or air quality currently present from area activities due to the brief work period (estimated two weeks) and small quantities of sediment removal (estimated 40 truckloads). Future operation of the modified boat basin and moorings area would not contribute additional air emissions beyond current levels.

Mitigations and Conclusions: The dredging of the boat basin and moorings area will not have significant impacts to the local climate and air quality. Therefore, no mitigation is necessary.

2.1.3 Noise

Affected Environment: Existing noise levels in the vicinity of the MWR Boat Ramp are typical of those normally associated with nearby land uses, commercial and recreational vessels on the Pasquotank River, and USCG activities. The primary sources of noise at the site are vehicular traffic, boat traffic on the river, aircraft traffic, wind, and waves. These noises are loudest during daylight hours, during the summer tourist season, and during storms.

Anticipated Impacts: The 'no action' alternative will have no significant impacts on noise. For the dredge alternative, heavy equipment and construction activity noises would result from dredging and transport of the dredged material to the landfill by barge and truck. There are no residences within 1,500 feet of the project and truck traffic will be routed through commercial areas. The noise from heavy equipment and construction activities would repel nearby fish, birds, and other wildlife. Fish and wildlife would likely relocate to a nearby habitat when the dredging activities begin, and will likely return after these activities are completed. These impacts are not significant because the noise will be consistent with other noise in the area and the dredging is short-term.

Dredging is expected to occur during the normal operating hours at the site. Noise is attenuated over distance according to the inverse square law, whereby each doubling of the distance, starting at 50 feet from the source, reduces the decibel level by 6 decibels. Dredging activities are also considered a temporary intrusion of noise. The impact of the project on noise is not significant because the noise will be consistent with other sources of noise nearby, the noise will be short-term, and there are no nearby residences.

Mitigations and Conclusions: During dredging activities, contractors should comply with all applicable federal and state laws on noise/hearing protection and abatement. No additional mitigation measures are proposed.

2.1.4 Hazardous Materials and Hazardous Waste

Affected Environment: Currently, the USCG Base Elizabeth City is identified as a Large Quantity Generator (LQG) of hazardous materials under Resource Conservation and Recovery Act regulations (US EPA ID No. NC2690308232). A LQG generator is defined as any entity, other than a household, that generates 1,000 kilograms (2,220 pounds) of hazardous waste or one (1) kilogram (2.2 pounds) of acutely hazardous waste per calendar month. The types of wastes generated are consistent with the Base operations involving materials utilized in the maintenance of vehicles and boats, buildings and industrial activities including paints, paint removers, petroleum, oil, lubricants, greases, solvents, car and boat engine batteries, and fluorescent lamps. Hazardous and non-hazardous wastes are collected and stored at several satellite collection points. Wastes including used oil (motor oil, lubricants, and grease); universal wastes (fluorescent lamps and used batteries); blast media, and paint waste. A 90-day hazardous waste storage area is used to store collected materials prior to final off-site disposal.

Anticipated Impacts: There would be no anticipated impacts from the 'no action' alternative. For the proposed dredging, the excavated sediment material is not characterized as a hazardous waste. Fuel and other petroleum products contained within the dredging/construction equipment could be released during the onsite activities (via leaks or accidents). However, these releases are not anticipated to be significant.

Mitigations and Conclusions: While releases from the dredging/construction equipment are not anticipated to be significant, the contractor should submit a spill prevention and response plan to the USCG covering all regulated materials brought to the project site. The plan should indicate how the contractor would manage their petroleum storage and respond to petroleum releases, and to releases of any other regulated materials or substances brought to the project site.

2.2 Natural Environment

The site was inspected by an environmental scientist from AECOM on May 7, 2014. A photographic log of the MWR Boat Ramp was completed during the survey and is included in **Appendix A**.

2.2.1 Terrestrial Environment

Affected Environment: The USCG Base Elizabeth City is located in the coastal plain of northern North Carolina along the southern shore of the Pasquotank River, and the MWR Boat Ramp is located at the north end of the Base's shoreline. A large portion of the Base is covered in grass areas that are maintained by mowing, while the shoreline is predominantly developed with roadways, paved parking lots and buildings.

Anticipated Impacts: There would be no anticipated impacts from the 'no action' alternative. For the proposed dredging, since the impacts are limited to the boat basin and moorings area, no impacts to the terrestrial environment are anticipated for this preferred alternative.

Mitigations and Conclusions: The dredging of the boat basin and moorings area will not have significant impacts to the terrestrial environment because no special terrestrial resources are present. No mitigation is necessary.

2.2.2 Water Resources and Aquatic Environment

Affected Environment – Surface Water: The USCG Base Elizabeth City is bordered to the east/northeast by the Pasquotank River. Storm water from the developed portions of the Base flows to storm drain catch basins and drainage ditches, which discharge to the Pasquotank River. Storm water runoff from undeveloped portions of the Base is allowed to infiltrate into the soils and allowed to naturally runoff via sheet flow into Pasquotank River.

Anticipated Impacts: There are no anticipated impacts from the 'no action' alternative. Minor impacts to aquatic vegetation and water resources will occur from the dredging activities. However, based on review of the North

Carolina OneMap Habitat Map, the project area is not located in the vicinity of a designated submerged aquatic vegetation area. Additionally, no threatened and endangered aquatic plant species are recorded in the proposed project area. It is anticipated that aquatic vegetation will become reestablished in the dredged area following the completion of the dredging activities. Minor impacts to fish species will also occur from the dredging activities. According to the United States Fish and Wildlife Service (USFWS) species list for Pasquotank County and the North Carolina Natural Heritage Program (NC NHP) species list for the Elizabeth City quadrangle, two federally-listed endangered species of fish (i.e., Shortnose sturgeon and Atlantic sturgeon) may be present in the proposed project area. The fish in the area will temporarily move during the dredging activities. It is anticipated that the fish will return to the area following the completion of the dredging activities.

During dredging and dewatering activities, there is potential for sediment from the dredging area to be temporarily suspended in the water column of the Pasquotank River. Since the project is confined to a small area, the extent of impact is anticipated to be limited to the immediate area of the dredging activities.

Mitigation and Conclusions: The sediment removal alternative is proposed to remove approximately 755 cubic yards of material. The dredging will be performed from a floating plant and not from land based operations. The sediment will be temporarily stored on a barge in the Pasquotank River for dewatering. Best Management Practices (BMPs), such as an impermeable floating turbidity curtain, will be used to keep the sediments from leaving the dredging area and entering the main channel of the Pasquotank River. The curtain will be a full depth floating design with weighted bottom edge, and it will remain in place in accordance with permit conditions or for a period of 48 hours after completion of dredging, whichever occurs longer. The dredge material will be dewatered on the barge within the turbidity curtain. The sediment material has been tested and characterized as non-hazardous material; therefore, the proposed project will not cause the degradation of water quality. Appropriate permits will be obtained for the discharge for the dewatering process, if required. Other appropriate BMPs, such as controlling the rate of dredging, that are required by Clean Water Act permits will be used in the dredge area, as needed, to minimize the amount of silt that becomes suspended. All water quality BMPs required by permits will be adhered to. Additionally, to avoid potentially adverse impacts on life cycles of anadromous fish, the proposed project is expected to occur outside of the anadromous fish spawning moratorium (February 15 to June 30).

2.2.3 Floodplains and Costal Zone

Affected Environment: The dredging location is in the Pasquotank River and by definition is in the floodplain. The project is also located in the Coastal Management Zone and within a North Carolina Division of Coastal Management (NC DCM) area of environmental concern.

Anticipated Impacts: The 'no action' alternative will have no impacts to floodplains and the coastal zone. The USCG determined that the project is consistent with the Coastal Zone Management Act (CZMA) and North Carolina's Coastal Zone Management Program (CZMP). AECOM, on behalf of the USCG, prepared and submitted a Federal Consistency Determination (see documentation in **Appendix C**) to the NC DCM on January 12, 2015. In a letter dated March 19, 2015, the NC DCM concurred with the determination stating "DCM has reviewed the submitted information pursuant to the management objectives and enforceable policies of Subchapters 7H and 7M of Chapter 7 in Title 15A of the North Carolina Administrative Code and concurs that the proposed Federal activity by the US Coast Guard is consistent, to the maximum extent practicable, with North Carolina's certified coastal management program." A copy of the NC DCM concurrence letter is included in **Appendix C**.

Mitigations and Conclusions: The dredging will not have significant impacts to floodplains, and no floodplain mitigation is necessary. The project is consistent with the CZMA and North Carolina's CZMP, and no mitigation is necessary.

2.2.4 Wetlands

Affected Environment: Reviews of the USFWS National Wetlands Inventory Map and the North Carolina Coastal Wetlands Map were completed to determine the potential presence of wetlands. The site was inspected to verify

the maps and to determine if any wetlands existed on the MWR Boat Ramp APE. The USFWS National Wetlands Inventory Map did not identify wetlands in or near the project APE. However, the northern and northwestern portions of the project APE as well as the remainder of the Pasquotank River are classified as Estuarine and Marine Deepwater (E1UBL) habitats. The North Carolina Coastal Wetlands Map indicated that there are no coastal wetlands in or near the project APE. No wetlands were observed during the inspection of the MWR Boat Ramp.

Anticipated Impacts: The 'no action' alternative will have no impacts on wetlands. For the dredge alternative, no wetlands exist in the APE and there will be no wetland impacts.

Mitigations and Conclusions: Wetlands will not be affected and no mitigation is necessary.

2.2.5 Prime and Unique Farmlands

Affected Environment: The Farmland Protection Policy Act (7 U.S.C. 4201, et seq.) is intended to preserve prime farmland for agricultural purposes whenever possible. The project is located within the Pasquotank River, which is not prime or unique farmland.

Anticipated Impacts: The 'no action' alternative will have no impacts on prime and unique farmlands. For the dredge alternative, no prime or unique farmlands exist in the dredge area or on-shore at the USCG Base Elizabeth City; therefore, there will be no impacts to prime or unique farmlands.

Mitigations and Conclusions: Prime and unique farmlands will not be affected and no mitigation is necessary.

2.2.6 Threatened or Endangered Species

Affected Environment: A list of State and Federally-listed rare, threatened, and endangered species (TES) that are recorded from within the APE and/or surrounding area was obtained online from the USFWS Threatened and Endangered Species and Species of Concern for Pasquotank County and the NC NHP Heritage Data Search for the Elizabeth City Quadrangle. **Table 1** summarizes the TES recorded from the online searches. The table also lists the species status, habitat requirements, and if the habitat is present at the project site. In addition, a NC NHP Information Request on the distribution of State-listed TES, a USFWS Section 7 Consultation Request for Federally-listed TES, and a National Marine Fisheries Service (NMFS) Section 7 Consultation Request for the sturgeon species were completed by AECOM (see documentation in **Appendix D**).

Table 1 – Summary of Records of Federal and State Rare, Threatened, and Endangered Species

Species (1, 2)	State Status (1)	Federal Status (2)	Habitat (1, 2)	Habitat Present
Bald Eagle (<i>Haliaeetus leucocephalus</i>)	T	BGPA	Mature forests near large bodies of water (for nesting); lakes and sounds (for foraging)	Yes
Red Knot (<i>Calidris canutus rufa</i>)	-	P	Migration stops along the Atlantic coast; principally found in marine and estuarine habitats; main food source is horseshoe crab eggs	Yes
Shortnose Sturgeon (<i>Acipenser brevirostrum</i>)	E	E	Anadromous species that prefers near shore marine, estuarine, and riverine habitat of large river systems; migrate periodically into faster moving fresh water areas to spawn	Yes
Atlantic Sturgeon (<i>Acipenser oxyrinchus</i>)	SC	E	Anadromous species; migrate from estuarine and marine waters into freshwater in the spring and early summer to spawn; spawn in moderately flowing water in deep parts of large rivers; sub adults and adults live in coastal waters and estuaries	Yes
American Eel (<i>Anguilla rostrata</i>)	-	FSC	Catadromous species; mature eels migrate from brackish and freshwater habitats in streams, large rivers, and estuaries to the ocean to spawn. Larvae develop in the ocean and the eel migrates upstream to estuaries and freshwater habitats as it matures	Yes
West Indian Manatee (<i>Trichechus manatus</i>)	-	E	Warm waters of estuaries and river mouths	Yes
Grassleaf Arrowhead (<i>Sagittaria weatherbiana</i>)	-	FSC	Fresh or brackish marshes, stream banks, and wet depressions	No
White Doll's-daisy (<i>Boltonia asteroides</i> var. <i>glastifolia</i>)	SR-O	-	Clay-based Carolina bays, marshes, savannas	No
Slender Blue Iris (<i>Iris prismatica</i>)	SR-T	-	Bogs, marshes, and wet powerline clearings	No
Winged Seedbox (<i>Ludwigia alata</i>)	SR-P	-	Interdune ponds, marshes	No
<p>State Status: E = Endangered T = Threatened SC = Special Concern SR-O = Significantly Rare – Other SR-P = Significantly Rare - Peripheral SR-T = Significantly Rare - Throughout - = Not listed by NC NHP in Elizabeth City quad N/A = Not Applicable</p> <p>Federal Status: E = Endangered T = Threatened FSC = Federal Species of Concern BGPA = Bald and Golden Eagle Protection Act P = Proposed - = Not listed by USFWS in Pasquotank County N/A = Not Applicable</p>				
<p>(1) Source: North Carolina Natural Heritage Program, Heritage Data Search (accessed September and November 2014) at http://www.ncnhp.org/web/nhp/database-search. State Listed species are terrestrial and aquatic species within approximately one mile of the site.</p> <p>(2) Source: U.S. Fish and Wildlife Service, Endangered Species, Threatened Species, Federal Species of Concern, and Candidate Species for Pasquotank County (accessed online September and November 2014) at http://www.fws.gov/raleigh/species/cntylist/pasquotank.html</p>				

Anticipated Impacts: The 'no action' alternative will have no impacts on threatened or endangered species. Based on site observations and habitat requirements, there is a potential for listed species protected under the Endangered Species Act to be impacted by the proposed action under the dredge alternative (i.e., Shortnose sturgeon and Atlantic sturgeon). The USFWS Section 7 determination concluded that the preferred alternative to dredge the boat basin and moorings "is not likely to adversely affect any federally-listed endangered or threatened species, their formally designated critical habitat, or species currently proposed for listing."

However, the USFWS response recommended the following actions to mitigate impacts to aquatic resources:

- "[T]he Service is concerned about the potential impacts the proposed action might have on aquatic species. Aquatic resources are highly susceptible to sedimentation. Therefore, we recommend that all practicable measures be taken to avoid adverse impacts to aquatic species, including implementing directional boring methods and stringent sediment and erosion control measures. An erosion and sedimentation control plan should be submitted to and approved by the North Carolina Division of Land Resources, Land Quality Section prior to construction. Erosion and sedimentation controls should be installed and maintained between the construction site and any nearby down-gradient surface waters. In addition, we recommend maintaining natural, vegetated buffers on all streams and creeks adjacent to the project site."

The NC NHP responded to USCG's request letter the "NC NHP database does not show any records for rare species, important natural communities, natural areas, or conservation/managed areas within the proposed project area." However, the NC NHP database shows element occurrence records for the Shortnose sturgeon, Atlantic sturgeon, and Slender Blue Iris within one mile of the project site. The element occurrence records for Shortnose sturgeon and Atlantic sturgeon "are located in Albemarle Sound and North Carolina coastal waters (including Albemarle Sound), respectively." The element "occurrence record for Slender Blue Iris is from Hogs Creek in Pasquotank County." Impacts to the remaining State-listed species are not anticipated because suitable habitat is not present in the APE.

The NMFS responded to the USCG's request for consultation stating that "NMFS believes that any effects of the proposed project on shortnose sturgeon are extremely unlikely to occur because this species is not likely to be present in the project area." With regard to the Atlantic sturgeon, the NMFS stated "We have identified a potential route of effects resulting from physical impacts of dredging to Atlantic sturgeon but concluded that Atlantic sturgeon are not likely to be adversely affected by the proposed action." A potential route of effects resulting from the temporary loss of access to the project site for foraging or resting because of dredging activities was also identified; however, the NMFS determined that "all foraging and resting effects will be discountable as no foraging or resting habitat currently exists in the area due to the shallow depths and vessels' frequent use of the area." The NMFS concluded that "the proposed action is not likely to adversely affect listed species and critical habitat under NMFS's purview."

Mitigation and Conclusions: To avoid potentially adverse impacts on life cycles of anadromous fish (i.e., Shortnose sturgeon and Atlantic sturgeon), the proposed project will occur outside of the anadromous fish spawning moratorium (February 15 to June 30). Additionally, to avoid potential impacts to aquatic species, an impermeable floating turbidity curtain with a weighted bottom edge will be used to keep the sediments from leaving the dredging area and entering the main channel of the Pasquotank River. AECOM contacted the North Carolina Division of Land Resources, Land Quality Section to inquire about submitting an erosion and sedimentation control plan for the proposed project. The North Carolina Division of Land Resources, Land Quality Section stated that since the only work on-shore will be the loading of dredged material into trucks for transport to the landfill, which is not anticipated to disturb more than one acre of land, a state-approved erosion and sedimentation control plan is not required. However, erosion and sediment controls will be installed and maintained between the off-loading site and nearby down-gradient surface waters by the contractor.

2.3 Socioeconomic Environment

2.3.1 Land Use and Zoning

Affected Land Use and Development Environment: The USCG Base Elizabeth City is located approximately 3.5 miles southeast of Elizabeth City, along Weeksville Road / Highway 344 (formerly Highway 34), in Pasquotank County, North Carolina. Land records indicate that the Base sits on an approximately 748-acre parcel, owned by the USCG. The Base is located on the south shore of the Pasquotank River, which connects to the Albemarle Sound and is part of the Intracoastal Waterway. The MWR Boat Ramp is located on the north end of the Base's shoreline, in a portion of river tidal flats. The Base can be accessed either from the main gate off Weeksville Road on the north end of the Base, or from a second entrance off Consolidated Road on the south end of the Base. The MWR Boat Ramp can be approached by water from the northeast or by land via Moukawsher Drive along the shoreline.

The USCG Base Elizabeth City is classified as "Institutional & Public" in recent (December 2011) Pasquotank County land use plans. The surrounding areas to the west, south, and southeast are classified as "Agricultural"; small areas classified as "Forestry" and "Commercial" are also present to the west and south. The area to the north-northwest of the Base is classified as "Elizabeth City Planning Jurisdiction." The Elizabeth City land use plan (December 2011) depicts the area to the north-northwest of the Base as "Undeveloped," "Low Density Residential," "High Density Residential," and "Public & Institutional".

Anticipated Impacts: The 'no action' alternative will have no impacts land use and zoning. The proposed depth improvements for the MWR Boat Ramp will not affect land use, zoning, existing or future development patterns, or city-approved land use and development plans because there will be no changes in land use.

Mitigations and Conclusions: The project will not affect land use and zoning. No mitigation is necessary.

2.3.2 Socioeconomic Environment

Affected Environment: The USCG Base Elizabeth City is located in Pasquotank County, North Carolina. According to United States Census Bureau data, the county supported a population of 39,981 residents in 2013, with a median age of 36.3 (2008-2012) and a mean household income of \$60,713 (2008-2012). The County racial mix in 2013 was estimated at about 58.1 percent white and 37.8 percent black or African American, with about 4.4 percent of Hispanic origin.

According to the 2008-2012 American Community Survey 5-Year Estimates, the unemployment rate in Pasquotank County was 8.1 percent, which is above the statewide average of 6.6 percent. Educational, health care, and social assistance services accounts for 28.5 percent of all jobs in the county, which is above the statewide average of 23.4 percent. The county also supports retail trade (12.6 percent); arts, entertainment and recreation, and accommodation and food services (10.1 percent); construction (6.9 percent); and manufacturing (6.6 percent).

Anticipated Impacts: The 'no action' alternative will have no impacts on the socioeconomic environment. The proposed depth improvements are not expected to have a material impact (adverse or otherwise) on the existing socio-economic environment. In total, it is anticipated that statistics reflective of the current socio-economic condition of Elizabeth City and Pasquotank County will be unchanged by the proposed alternative.

Mitigations and Conclusions: The project will not affect the socioeconomic environment and no mitigation is necessary.

2.3.3 Community

Affected Environment: The USCG Base Elizabeth City was built in 1940 and consists of over 100 buildings which have been constructed over the lifetime of the Base. The Base is the largest USCG Air Station in the nation, and contains a number of commands including Air Station Elizabeth City, Aircraft Repair and Supply Center, Aviation Technical Training Center, Support Center Elizabeth City, and Small Boat Station Elizabeth City. Base Elizabeth City provides a variety of mission support services to units and personnel, including administration and personnel

management, health care services, supply, procurement and warehousing, industrial services, facilities maintenance, computer/electronics support as well as morale and recreational services. Base Elizabeth City coordinates and provides regional mission support activities within the Coast Guard Fifth Coast District. Since 1995 the Air Station has averaged over 360 search and rescue missions each year.

Anticipated Impacts: Given the apparent need for dredging, the 'no action' alternative would be expected to have an impact on the community. During wind-induced periods of low water and due to sediment deposition, there is inadequate clearance depth at the MWR Boat Ramp for launching USCG SPC and RB-S vessels. As a result, the USCG is routinely forced to launch their vessels at a public boat launch a number of miles upriver, which results in up to an hour delay for mission response.

The dredging activities proposed at the MWR Boat Ramp are not expected to have long-term adverse impact on the existing community. Short-term impacts, including removal of dredge material by barge and truck, would be localized and are not anticipated to be significant. Construction activity will be similar to any construction project in the area. Any damage that occurred to the roads as a result of the project would be repaired by the contractor conducting the work.

Mitigations and Conclusions: Construction activities should be limited to normal working hours. Truck traffic will be along existing public and government owned roadways. With these standard mitigation measures the project should have minimal to no impact on the community.

2.3.4 Infrastructure/Utilities

Affected Environment: Entry to the Base can be gained from either Weeksville Road through the main gate on the north end of the Base, or from Consolidated Road through the secondary gate on the south end of the Base. Access to the MWR Boat Ramp, located on the north end of the Base's shoreline, is via Moukawsher Drive along the shoreline. The access roads to and from the Base are improved (concrete/asphalt). Utilities associated with the MWR Boat Ramp include a storm sewer outfall west of, and outside the west rubble mound breakwater (not within the protected harbor area) and underground electric. The utilities associated with the MWR Boat Ramp are remotely located from the proposed dredging area.

Anticipated Impacts: The 'no action' alternative will have no impacts on infrastructure or utilities. The proposed project will have no adverse impact on existing, operational infrastructure, or utilities. Underground utilities will be identified and avoided or restored after dredging. Existing roads between the site and the East Carolina Regional Landfill (1922 Republican Road, Aulander, Bertie County, North Carolina) are capable of supporting heavier truck traffic for disposal of the dredged material as long as the contracted hauler adheres to the State load limits (80,000 pounds for four or more axles). Any impacts from trucking will be minimal and short-term.

Mitigations and Conclusions: The project will not affect infrastructure. No mitigation other than standard construction practices to identify, avoid, and restore infrastructure is necessary.

2.3.5 Public Service/Public Health and Safety

Affected Environment: The North Carolina Wildlife Resources Commission (NC WRC) tracks information on boating safety, including accidents and fatalities. For 2013, state reports indicated that about 50 boating accidents occurred per 100,000 registered boats, up slightly from 2012 levels (48.79 accidents per 100,000 registered boats). Fatal boating accidents were down slightly since 2012, decreasing from 7.43 to 5.58 fatal accidents per 100,000 registered boats. Clearly, the ability of USCG personnel to perform their work and achieve their mission is dependent on their ready access to all available seaworthy vessels, with harbors and moorings of sufficient depth to support mission requirements. Other public safety (fire, police, health care) are provided by the City and local hospitals.

Anticipated Impacts: The 'no action' alternative will have adverse impacts on public service, health, and safety because the ability of the USCG to carry out search and rescue missions from the USCG Base Elizabeth City will be impaired by shallow water at the boat basin and moorings. The proposed depth improvements will facilitate

USCG operations, and these operations promote public service, health and safety. The proposed dredging will not affect local fire, police, and health care services.

Mitigations and Conclusions: The project will have no adverse impacts to public service, health and safety. No mitigation is necessary.

2.3.6 Recreational Resources

Affected Environment: According to the NC WRC, the number of boats registered in-state has steadily decreased from 2009 to 2013, decreasing from 368,004 to 304,658. For Pasquotank County, the number of registered boats has decreased proportionally from 2,033 boats in 2009 to 1,707 registered boats in 2013.

Anticipated Impacts: Under the 'no action' alternative USCG response times might increase if alternative harbor facilities are required, which could impact recreational use. Additionally, the MWR Boat Ramp is used by Base military personnel for launching privately-owned recreational boats. The 'no action' alternative will also have adverse impacts on recreational use of the boat ramp by military personnel. The proposed depth improvements will not adversely impact recreational resources, but rather have a positive effect.

Mitigations and Conclusions: The project will not adversely affect recreational resources and no mitigation is necessary.

2.3.7 Environmental Justice

Affected Environment: Federal guidance indicates that environmental justice concerns may arise from impacts on the natural and physical environment, such as human health or ecological impacts on minority populations, low-income populations, and Native American tribes, or from related social or economic impacts. According to the United States Census Bureau data for Elizabeth City, the estimated 2013 population was 18,266 residents with a median age of 31 years and a racial makeup that is less diverse than the county or state as a whole. Racial breakdowns for 2010 identified a 0.4 percent Native American presence in Elizabeth City, which is less than the county or state as a whole.

US EPA records were evaluated using the Environmental Justice View web-tool. According to the web-tool, the nearest Superfund sites are located in Portsmouth and Suffolk, Virginia, and the nearest Brownfield sites are located in Hampton, Virginia and Williamston, North Carolina.

The dredging operation itself would only be expected to generate short-term impacts, related to noise and truck activity. With roads in close proximity to the site, trucks will not need to navigate through residential areas to reach disposal sites.

Anticipated Impacts: The 'no action' alternative will have no impacts on environmental justice. The proposed project will have no adverse impact on any potential environmental justice area as this concept is currently applied.

Mitigations and Conclusions: The project will not affect environmental justice and no mitigation is necessary.

2.4 Cultural Resources

Section 106 of the National Historic Preservation Act requires that impacts from federal undertakings on archaeological and architectural resources (i.e., cultural resources) that are listed or have been determined eligible for listing in the National Register of Historic Places in the APE, be taken into account in project planning. If adverse effects result, Section 106 requires that mitigation measures mutually agreeable to the lead agency and the relevant State Historic Preservation Office (SHPO) be implemented. The USCG provided a project-specific informational letter to the North Carolina SHPO dated July 18, 2014 that summarized the results of a literature search and background review of recorded cultural resources for the proposed actions at the USCG Base Elizabeth City, requesting their concurrence with the findings.

2.4.1 Prehistoric and Historic Context

Affected Environment:

Archaeological Resources: No archaeological investigations have been completed in the APE. There are no National Register-listed or -eligible, or potentially eligible (according to the SHPO Study List) terrestrial or underwater archaeological sites located within or immediately adjacent to the APE. No shipwrecks have been recorded in the MWR boat ramp and moorings APE and no historic properties have been recorded at the East Carolina Regional Landfill.

Architectural Resources: No historic architectural surveys have been completed in the APE. There are no historic architectural resources located within or immediately adjacent to the APE. Three historic architectural resources have been identified within one mile of the APE, including one site that has been Determined Eligible and two potentially eligible sites on the SHPO Study List. The Thrun Hall Barracks is a site that has been determined National Register-eligible in 2010 and is located inland from the MWR Boat Ramp, approximately three-quarters of a mile to the southeast of the APE. The Thrun Hall Barracks were subsequently photo documented and demolished upon the SHPO's approval. The John Hollowell House (PK0972) and the C.W. Hollowell House (PK0474) are on the Study List and are located inland from the MWR Boat Ramp, approximately one mile to the south-southwest of the APE.

Anticipated Impacts: The 'no action' alternative will have no impacts on cultural resources. In a letter dated August 19, 2014, the North Carolina SHPO concurred with AECOM's findings, stating "We have conducted a review of the project and are aware of no historic resources which would be affected by the project. Therefore, we have no comment on the project as proposed." The consultation letter submitted by AECOM and the North Carolina SHPO concurrence letter are included in **Appendix E**.

Mitigations and Conclusions: No cultural or historic resources will be affected by this project and no mitigation is necessary.

2.5 Summary of Potential Impacts

A summary of the potential impacts is presented in **Table 2**.

Table 2 - Summary of Potential Environmental Impacts

Potential Impacts	No Action Alternative	Dredging Boat Basin and Moorings at MWR Boat Ramp
Geology, Topography, Soils	None	Not significant. Approximately 755 cubic yards of sediment will be removed from the boat basin and mooring at the MWR Boat Ramp launch area. The sediment will be disposed of at a licensed landfill (East Carolina Regional Landfill).
Climate and Air Quality	None	Not significant. There will be minor and short-term emissions from dredging equipment and trucks similar to any construction project.
Noise	None	Not significant. Short-term impacts to commercial areas and nearby residences. Wildlife will likely relocate to a nearby habitat during the dredging.
Hazardous Materials	None	Not significant. Short-term potential accidental releases from construction equipment will be effectively mitigated by requirement for contractor's spill prevention and response plan.

Potential Impacts	No Action Alternative	Dredging Boat Basin and Moorings at MWR Boat Ramp
Terrestrial Environment	None	Not significant. No changes proposed. Short-term construction staging limited to USCG property paved parking areas.
Water Resources and Aquatic Environment	None	Not significant. Short-term and local minor disturbance of benthic habitat. Sediment re-suspension and transport will be effectively mitigated by construction practices and BMPs. The dredge material will be dewatered on the barge within the turbidity curtain. The sediment material has been tested and characterized as non-hazardous material; therefore, the proposed project will not cause the degradation of water quality. Comments received from the North Carolina Division of Water Resources (NC DWR) following the NC State Environmental Intergovernmental Review included a recommendation for the use of a turbidity curtain.
Floodplains and Coastal Zone	None	Not significant. Minor increase in flood storage. The proposed project is consistent with the CZMA and North Carolina's CZMP.
Wetlands	None	No impact. Wetlands not present.
Prime and Unique Farmlands	None	No impact. Farmland not present.
Threatened or Endangered Species	None	Not significant. Short-term and local minor impact to aquatic habitat. Threatened or endangered species present within the project APE (i.e., Atlantic sturgeon and Shortnose sturgeon) will likely relocate to nearby habitat during the dredging, and dredging will not be performed during the moratorium. The USFWS Section 7 determination concluded that the preferred alternative to dredge the boat basin and moorings "is not likely to adversely affect any federally-listed endangered or threatened species, their formally designated critical habitat, or species currently proposed for listing." The NMFS concluded that "the proposed action is not likely to adversely affect listed species and critical habitat under NMFS's purview."
Land Use and Zoning	None	No impact. No change in land use or zoning.
Socioeconomic Environment	None	No impact.
Community	None	Not significant. Short-term increase in truck traffic through commercial areas consistent with current traffic.

3 Statement of Environmental Significance

The dredging of the boat basin and moorings, to an 8-foot depth below MWL, is required to support the USCG Base Elizabeth City and its missions. The missions include search and rescue, environmental protection, and law enforcement. These missions are essential to public safety, recreational vessels, and commercial navigation.

Any impacts from the proposed dredging to the terrestrial, aquatic, and human environment would be localized and temporary and are not significant.

4 Summary of Mitigations

During construction activities, contractors should comply with all applicable federal and state laws on noise/hearing protection and abatement. Construction is expected to occur during the normal operating hours at the USCG Base Elizabeth City. Standard construction practices to identify, avoid, and restore infrastructure should be implemented. Truck traffic will be routed along existing roadways to the licensed East Carolina Regional Landfill in Aulander, North Carolina for disposal. Formal approval for disposal of the dredge material at the MWR Boat Ramp has been received from the landfill. Truckloads will be covered prior to transport and dredge material will be required to pass a paint filter test prior to disposal and landfill acceptance. To ensure the proposed dredge depth is not exceeded, pre- and post-dredge soundings will be taken by a registered surveyor to verify the sediment depth.

The dredging contractor will submit a spill prevention and response plan to the USCG covering all regulated materials brought to the project site. The plan will indicate how the contractor will manage their petroleum storage and respond to petroleum releases, and to releases of any other regulated materials or substances brought to the project site. The contractor will complete a daily report for each day of the onsite dredging activities. The daily report will document the weather; work hours; work in-place; inspections and tests conducted, and their results; dimensional checks; equipment and material checks; the mobilization and demobilization of construction equipment; materials delivered to the site; and any other pertinent noteworthy event.

The dredging will be performed from a floating plant and not from land based operations. BMPs, such as an impermeable floating turbidity curtain, will be used to keep the sediments from leaving the dredging area and entering the main channel of the Pasquotank River. The dredge material will be dewatered on the barge within the turbidity curtain. Appropriate permits will be obtained for the discharge for the dewatering process, if required. Other appropriate BMPs, such as controlling the rate of dredging, that are required by Clean Water Act permits will be used in the dredge area, as needed, to minimize the amount of silt that becomes suspended and transported downstream. All water quality BMPs required by permits will be adhered to.

Since the only work on-shore will be the loading of dredged material into trucks for transport to the landfill, which is not anticipated to disturb more than one acre of land, a state-approved erosion and sedimentation control plan is not required. However, erosion and sediment controls will be installed and maintained between the off-loading site and nearby down-gradient surface waters by the contractor. Additionally, to avoid potentially adverse impacts on life cycles of anadromous fish, the proposed project will occur outside of the anadromous fish spawning moratorium (February 15 to June 30). Required permit(s) may also specify BMPs or mitigation measures. Contractors will be required to comply with these permit conditions.

5 Regulatory Requirements

The CZMA requires that Federal agency activity within or outside the coastal zone that affects any land or water use or natural resource of the coastal zone shall be carried out in a manner which is consistent to the maximum extent practicable with the enforceable policies of the approved State management programs. The preferred alternative, dredging the boat basin and existing moorings is not anticipated to adversely affect the coastal zone resources of North Carolina's CZMP other than as evaluated elsewhere within this EA. AECOM prepared a Federal Consistency Determination, which was submitted on January 12, 2015, seeking concurrence from the NC DCM. A copy of the NC DCM concurrence letter (Consistency Determination #CD15-004; NC DCM Project #20150003) is included in **Appendix C**. Compliance with applicable federal environmental regulatory requirements and Executive Orders pertaining to air, water, noise, biota, floodplains, wetlands, coastal zone, waste management, transportation, and cultural and historic resources, etc. is a requirement of contractors working on this project, which have been discussed and presented in this EA. In addition, AECOM, on behalf of the USCG, completed a Joint United States Army Corps of Engineers (USACE) and NC DWR Pre-Construction Notification (PCN) Application. The USACE Regional General Permit (GP 198200277) and the NC DWR approval of Water Quality General Certification Number 3894 have been received for this project. Copies of the USACE and NC DWR permits are included in Appendix F.

AECOM, on behalf of the USCG, submitted a special waste profile and laboratory analytical data to Republic Services and was approved by the East Carolina Regional Landfill to dispose of the dredge material from the project at the existing landfill. A copy of the Special Waste Department Decision is included in **Appendix F**.

The project will not affect state-designated environmental areas or wetlands. The project will not affect historic or cultural resources.

6 Public Involvement

During the preparation of this EA, several federal, state, and local agencies and organizations were consulted. In addition to consultation letters discussed in this EA, the USCG submitted the Draft EA to the state and federal agencies and organizations consulted during its completion for review. In addition, copies of the Draft report were made available to the local community for review at the East Albemarle Regional Library, Pasquotank County Branch. A notice was placed in the *Daily Advance* to inform the community about the availability of the Draft EA. Coordinating agencies and the public were provided a 30-day review period and encouraged to provide comments.

After receiving and considering the comments on the Draft EA from the public and coordinating agencies, the USCG issued this Final EA and Finding No Significant Impact (FONSI). As provided by NEPA and as referenced in COMDTINST M16475.1D, the FONSI for the preferred alternative will be made available to the public for a period of not less than 30 days before the final determination is made and the action is implemented. Any necessary consultations and permits will be conducted and obtained during this period. No on-site activities related to the preferred alternative will be initiated until the environmental review process has been completed.

7 Conclusion

Two alternatives were considered for evaluation in this EA: no action and dredging to a depth of 8 feet below MWL. The EA was performed in accordance with NEPA, *Procedures for Considering Environmental Impacts* (Department of Transportation 1979, rev. 1982, rev. 1985) Order 5610.1C, and COMDTINST M16475.1D.

Based on the results presented in this EA, the preferred alternative is dredging sediment to a depth of 8 feet below MWL and transporting the sediment via truck for disposal at a licensed landfill. The potential adverse environmental impacts presented during this evaluation can either be prevented or reduced to insignificant levels using the mitigation measures presented in this EA. The results of this EA indicate that implementation of the proposed action will not cause significant changes in the quality of the human and natural environment, supporting a FONSI.

8 Persons and Agencies Contacted

Ms. Allison Weakley, NCDENR, North Carolina Office of Land and Water Stewardship, North Carolina Natural Heritage Program, 1601 Mail Service Center, Raleigh, North Carolina 27699

Ms. Anne Deaton, North Carolina Division of Marine Fisheries, Habitat Protection Section, 3441 Arendell Street, Morehead City, North Carolina 28557

Mr. Daniel Govoni, Federal Consistency Coordinator, NC DCM, 400 Commerce Avenue, Morehead City, North Carolina 28557

Mr. Josh Pelletier, USACE, Washington Regulatory Field Office, 2407 W. 5th Street, Washington, North Carolina 27889

Ms. Maria Dunn, NC WRC, 1721 Mail Service Center, Raleigh, North Carolina 27699

Mr. Patrick McClain, North Carolina Division of Land Resources, Land Quality Section, Washington Region, 943 Washington Square Mall, Washington, North Carolina 27889

Mr. Pete Benjamin, USFWS, Raleigh Field Office, Post Office Box 33726, Raleigh, North Carolina 27636

Ms. Renee Gledhill-Earley, North Carolina Department of Cultural Resources, State Historic Preservation Office, 4617 Mail Service Center, Raleigh, North Carolina 27699

Mr. Robert Tankard, North Carolina Division of Water Resources, Water Quality Regional Operations Section, 943 Washington Square Mall, Washington, North Carolina 27889

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Figures

Figure 1 – Site Location Map

Figure 2 – Aerial Photograph



Map Location



Site Location Map

MWR Boat Ramp
USCG Base Elizabeth City, North Carolina
Elizabeth City NC Topographic Quadrangle (1983)

0 1,000 2,000
Feet

June 2014

60321114

Figure 1

AECOM

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Scale: Not To Scale

(Source: Google Earth© 2014)

Dredge Site - Aerial Photograph

US Coast Guard
Base Elizabeth City – MWR Boat Ramp
Elizabeth City, NC

AECOM

Figure 2

www.aecom.com

Appendix A.

Photographic Survey Log

Facility Name: United States Coast Guard	Site Location: MWR Boat Ramp – USCG Base Elizabeth City, NC	Project No. 60321114
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Photo No. 1	Date: 05/7/14	
Direction Photo Taken: North		
Description: View from the southern side of the shore (concrete pavement approach) of the concrete ramp, docks, navigation aids, and light pedestals.		

Photo No. 2	Date: 05/7/14	
Direction Photo Taken: Northwest		
Description: View from the concrete pavement approach along the southern shoreline.		

Facility Name: United States Coast Guard	Site Location: MWR Boat Ramp – USCG Base Elizabeth City, NC	Project No. 60321114
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Photo No. 3	Date: 05/7/14
Direction Photo Taken: South	
Description: View looking south of the concrete ramp, docks, light pedestals, and mooring piles.	

A wide-angle photograph taken from a wooden dock, looking south towards a boat ramp. The water is dark blue with small ripples. In the middle ground, a concrete ramp leads to a sandy beach area. There are several buildings, including a small red-roofed structure, and a few people are visible on the beach. A small boat is in the water near the ramp. In the background, there are more buildings, a parking lot with a white truck, and a red and white striped tower. The sky is clear and blue.

Photo No. 4	Date: 05/7/14
Direction Photo Taken: North	
Description: View at the end of the concrete ramp of the docks, light pedestals, and mooring piles.	



Facility Name: United States Coast Guard	Site Location: MWR Boat Ramp – USCG Base Elizabeth City, NC	Project No. 60321114
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Photo No. 5	Date: 05/7/14
Direction Photo Taken: South-Southeast	
Description: View of the man-made beach located southeast of the MWR Boat Ramp.	



Photo No. 6	Date: 05/7/14	
Direction Photo Taken: North		
Description: View of the navigation aids at entry to the protected harbor area. Note two timbers low to water surface in the entry-way.		

Appendix B.

Proposed Alternative 100 Percent Design Drawings



PROJECT
LOCATION

CEU FIELD
OFFICE

MAIN GATE

ROUTE 163

VICINITY MAP

NOT TO SCALE

DISPOSAL SITE LOCATION MAP

0 5 10
SCALE IN MILES

0 400 800 1200
SCALE FEET

DRAWING INDEX

SHEET	TITLE
C-01	SITE PLAN
C-02	HYDROGRAPHIC SURVEY
C-03	DREDGE PLAN
C-04	DREDGE CROSS SECTIONS

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U. S. COAST GUARD
CIVIL ENGINEERING UNIT
CLEVELAND



USCG. CEU CLEVELAND
1240 EAST 9TH STREET
CLEVELAND, OH 44199-2060

ISSUE

MARK	DATE	DESCRIPTION
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A/E PROJECT NO: 60321114
CAD FILE NAME: C8257C01.DWG
DESIGNED BY: SJR
DRAWN BY: MJC
EDITED BY: MJC
CHECKED BY: TAP

SCALE: AS SHOWN PLOT SCALE: 1:1

SHEET TITLE

DREDGING AT MWR BOAT RAMP
CG BASE ELIZABETH CITY
ELIZABETH CITY NC
MWR BOAT RAMP
CIVIL
SITE PLAN

REVIEWED BY: J.W. NICKS PROJECT ENG.	REVIEWED BY: G.P. LODGE BRANCH CHIEF	REVIEWED BY: G.S. PLACZEK TECH. DIRECTOR
--	--	--

P.R. CARROLL, CDR.
APPROVING OFFICER

DATE

PROJECT NUMBER

3756419

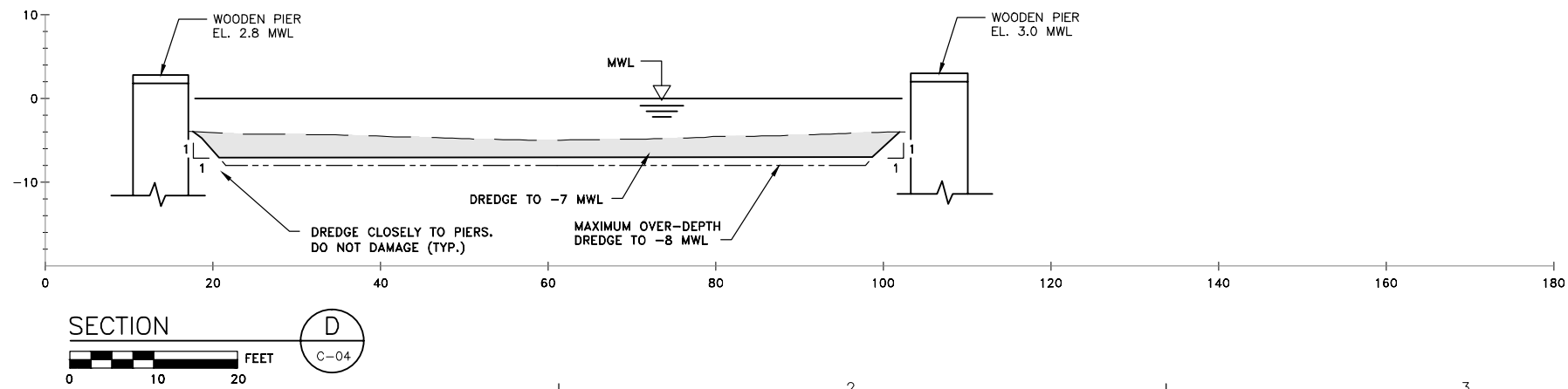
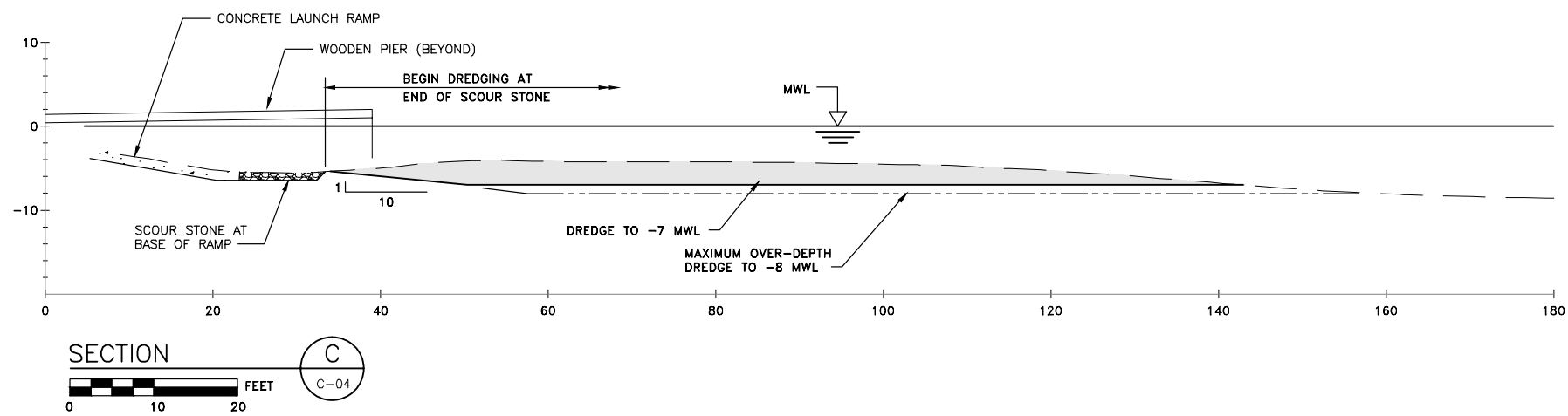
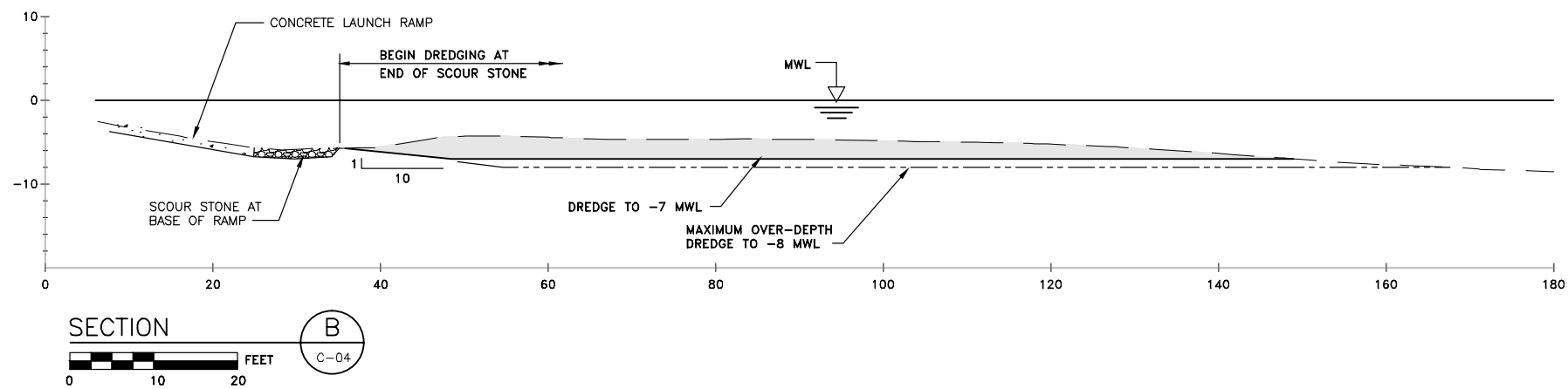
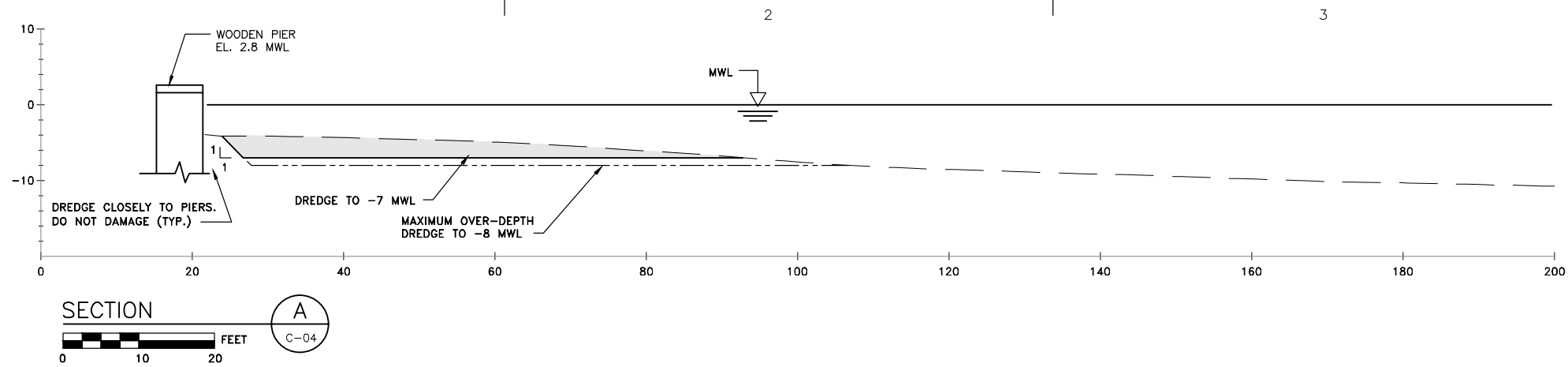
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C-01

DRAWING NUMBER

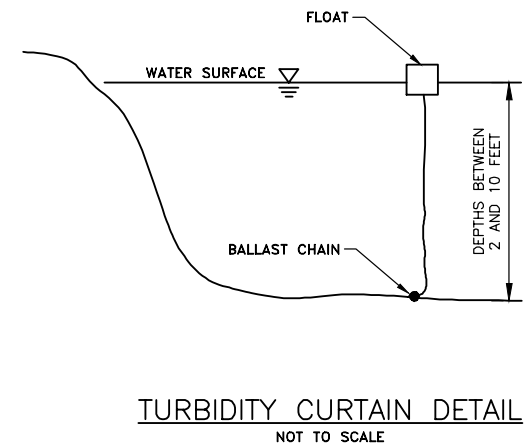
8257-D

SHEET 1 OF 4



LEGEND

- EXISTING TOP OF SEDIMENT
- PROPOSED BASE DREDGE ELEVATION
- - - MAXIMUM OVER-DEPTH DREDGE

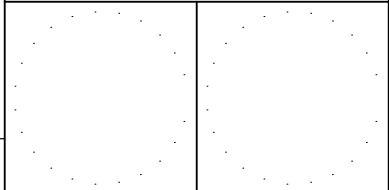


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CIVIL ENGINEERING UNIT
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ISSUE		
MARK	DATE	DESCRIPTION

A/E PROJECT NO: 60321114		
CAD FILE NAME: C8257C04.DWG		
DESIGNED BY: SJR		
DRAWN BY: MJC		
EDITED BY: MJC		
CHECKED BY: TAP		

SCALE: AS SHOWN PLOT SCALE: 1:1

SHEET TITLE

DREDGING AT MWR BOAT RAMP
CG BASE ELIZABETH CITY
ELIZABETH CITY NC
MWR BOAT RAMP
CIVIL
DREDGE CROSS SECTIONS

REVIEWED BY:	REVIEWED BY:	REVIEWED BY:
J.W. NICKS	G.P. LODGE	G.S. PLACZEK
PROJECT ENG.	BRANCH CHIEF	TECH. DIRECTOR

P.R. CARROLL, CDR.
APPROVING OFFICER

DATE

PROJECT NUMBER	DRAWING NUMBER
3756419	8257-D
DISCIPLINE/SHT NO	
C-04	SHEET 4 OF 4

Appendix C.

Coastal Zone Federal Consistency Determination

**U.S. Department of
Homeland Security**

**United States
Coast Guard**



Commanding Officer
United States Coast Guard
Civil Engineering Unit Cleveland

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11000

Dan Govani
Federal Consistency Coordinator
North Carolina Division of Coastal Management
400 Commerce Avenue
Morehead City, NC 28557-3421

Dear Mr. Govani:

In accordance with the federal consistency requirements of the Coastal Zone Management Act, 16 USC § 1456, and applicable regulations, 15 CFR § 930.36, the United States Coast Guard (USCG) is submitting this correspondence to fulfill State agency coordination requirements. The USCG proposes to complete maintenance dredging of the boat basin and moorings at the Morale, Welfare and Recreate (MWR) Boat Ramp at the USCG Base Elizabeth City located at 1664 Weeksville Road, Elizabeth City, Pasquotank County, North Carolina. The geographic location of the MWR Boat Ramp is latitude 36° 16' 14.72" north and longitude 76° 10' 40.31" west.

The USCG, Civil Engineering Unit (CEU) Cleveland, Environmental Section conducted a coastal zone management Federal consistency review of the proposed project and determined that the activities will be undertaken in a manner consistent, to the maximum extent practicable, with the enforceable policies of North Carolina's approved coastal management program.

This correspondence provides your office with a proposed project description provided in Enclosure (1). The basis of the coastal zone determination is summarized in Enclosure (2). The proposed project Site Location Map and other supporting figures are presented in Enclosure (3). Enclosure (4) contains the Dredge Plan. In accordance with State guidance, the USCG has enclosed 15 CDs of this submittal in PDF format (Enclosure 5).

The USCG requests your written concurrence with our consistency determination. If you have any questions, please contact Ms. Leilani Woods of my staff at (252) 335-6847. Alternatively, you may contact Ms. Jennifer Jones with our design firm (AECOM) at (919) 239-7150.

Sincerely,

A handwritten signature in blue ink, appearing to read "G. O. Carpenter".

Gregory O. Carpenter
Chief, Environmental Compliance
By direction of the Commanding Officer

Encl: (1) Proposed Project Description
(2) Basis of Determination

- (3) Figures 1-9
- (4) Dredge Plan – Existing Features
- (5) 15 CDs of this USCG Consistency Determination Submittal

Cc: Ms. Leilani Woods, Environmental Engineer, USCG Civil Engineering Unit
Cleveland
Ms. Jennifer Jones, Environmental Project Manager, AECOM

ENCLOSURE (1)

PROPOSED PROJECT DESCRIPTION

PROPOSED PROJECT DESCRIPTION

The U.S. Coast Guard (USCG) proposes to complete maintenance dredging of the existing boat basin and moorings at the Morale, Welfare and Recreate (MWR) Boat Ramp at the USCG Base Elizabeth City located at 1664 Weeksville Road, Elizabeth City, Pasquotank County, North Carolina. The facility location is shown on the Site Location Map in Enclosure (3) Figure (1). The Dredge Plan, which depicts the dredge limits, is provided in Enclosure (4).

The MWR Boat Ramp is located on the north end of the Base's shoreline, in a portion of the river tidal flats. The MWR Boat Ramp consists of a two-lane concrete ramp with a concrete pavement approach, and a launch dock centered in the boat ramp. Rubble mound stone breakwaters project approximately 100 to 125 feet from the shoreline on both sides of the launch ramp to form a protected harbor area for launching and retrieving boats. Timber docks line both sides of the boat basin. Mooring piles are in place along the west timber dock. Navigation aids are located near the entrance to the boat basin. The project area is bounded by a timber dock, a breakwater and the Pasquotank River to the north; a timber dock, a breakwater and the Pasquotank River to the east; a launch dock, boat ramp and grass covered shoreline to the south; and a timber dock, a breakwater, and grass covered shoreline to the west followed by the Pasquotank River.

The MWR Boat Ramp is utilized by Base military personnel for launching privately-owned recreational boats and for launching USCG vessels stationed at the Building 43 moorings. The purpose of this project is to dredge the boat basin and moorings to a required depth below mean water level due to fluctuating conditions at the site. Water depth in the boat basin and moorings is often reduced by heavy winds from the north, which push water from the boat basin and down the Pasquotank River. This wind effect decreases the depth of the water and encumbers the USCG's ability to utilize the existing boat ramp for both operational missions and recreational purposes.

The MWR Boat Ramp is a previously dug man-made basin and the proposed dredging will occur entirely within the previously disturbed footprint of the boat basin and moorings. However, the USCG has no record of the as-built drawings indicating the original dredge depth. While the dredging required for the original construction of the MWR Boat Ramp likely disturbed the sediments to a depth proximate to the proposed dredge depth, the exact depth of the original dredging is not documented. The proposed project represents the first maintenance dredging of the MWR Boat Ramp since its construction. The construction date of the boat ramp is unknown.

The proposed project involves mechanical dredging of the boat basin and moorings to a maximum depth of 8 feet below mean water level (includes one foot over dredge depth). Mean water level corresponds to elevation -0.44 North American Vertical Datum 1988 (NAVD88). Therefore, dredging will proceed to -8.44 feet NAVD88. The calculated dredge volume for the MWR Boat Ramp is 755 cubic yards of sediment. The proposed dredging project will only temporarily impact open waters (an area totaling approximately 0.18 acres) and will occur entirely within the existing boat basin (limited to an area approximately 150 feet from the shoreline).

The USCG has completed a pre-design hydrographic survey, sediment and elutriate sampling and analysis, and disposal site determination for the MWR Boat Ramp. Major items of work that

PROPOSED PROJECT DESCRIPTION

will be involved in the maintenance dredging of the MWR Boat Ramp boat basin and moorings at Base Elizabeth City and off-site disposal of the dredged material include the following:

1. An approved registered surveyor will perform Pre-dredge and Post-dredge (before and after) soundings of area to be dredged including side slopes. Drawings will be prepared showing the before and after sounding data. Soundings will be taken within 15 days prior to the start of the dredging and within 3 days after completing all dredging. Soundings will be taken at maximum 10-foot spacing along transects spaced no greater than 10 feet on center across the proposed dredge area (including side slopes). Progress soundings or sweepings will also be taken behind the dredge as the work progresses.
2. The registered surveyor will calculate the dredge quantity based on the before and after soundings. The boat basin and moorings will be dredged to a maximum depth of 8 feet below mean water level (includes one foot over dredge depth). Additional dredging may be necessary to remove any shoaling or sedimentation that has occurred since the May 7 to 9, 2014 hydrographic survey included in the Design Drawings, and the "Before" hydrographic survey.
3. The dredging will be accomplished using mechanical means. The dredging will be performed from a floating plant and not from land based operations. The dredge material will be deposited on barges, dewatered from the barge within the turbidity curtain, transported by barge to an unloading site, offloaded to trucks, and transported by truck to the approved East Carolina Regional Landfill in Aulander, North Carolina. The dredge sediment material has been characterized as a non-hazardous waste and has been approved for disposal at the East Carolina Regional Landfill.
4. Prior to commencing dredging operations, the proposed area for dredging will be enclosed with an impermeable floating turbidity curtain. The curtain will be of a full depth floating design with weighted bottom edge. The turbidity curtain will remain in place until dredging operations are completed and the final contracted depths have been verified. The turbidity curtain will remain in place in accordance with permit conditions or for a period of 48 hours after completion of dredging, whichever occurs longer.

Preliminary review indicates the dredging of the boat basin and mooring area will require approval from the United States Army Corps of Engineers (USACE) and the North Carolina Department of Environment and Natural Resources (NCDENR) Division of Water Resources (DWR). The USACE and DWR have simplified the permitting process by allowing the submittal of a joint permit application to both agencies. The joint permit application was submitted to the USACE and DWR on September 24, 2014.

The dredge material will be disposed of at the East Carolina Regional Landfill located at 1922 Republican Road in Aulander, Bertie County, North Carolina. The landfill is approximately 63 miles from Base Elizabeth City. This landfill is a licensed and approved Subtitle D landfill permitted to accept special waste. Formal approval for disposal of the dredge material at the MWR Boat Ramp has been received from the East Carolina Regional Landfill.

ENCLOSURE (2)

BASIS OF DETERMINATION

BASIS OF DETERMINATION

**North Carolina Administrative Code
Title 15a, Chapter 7, Coastal Management
Subchapter 7H – State Guidelines for Areas of Environmental Concern**

Management Objectives

1. Section .0205 Coastal Wetlands

The USCG reviewed the U.S. Fish and Wildlife Service (USFWS) National Wetlands Inventory Map which did not identify wetlands in or near the project site. However, the northern and northwestern portions of the project site and the remainder of the Pasquotank River are classified as Estuarine and Marine Deepwater (E1UBL), as shown in Enclosure (3) Figure (3). The USCG reviewed the North Carolina Coastal Wetlands Map, which indicated that there are no coastal wetlands in or near the project site, as shown in Enclosure (3) Figure (2). The proposed project will not have an effect on coastal wetlands, and is therefore consistent with this management objective.

2. Section .0206 Estuarine Waters

The proposed project site is located in the estuarine waters of the Pasquotank River, Elizabeth City, Pasquotank County as shown in Enclosure (3) Figure (1). The proposed project will not impact the biological, social, aesthetic, and economic value of estuarine waters. The proposed project is consistent with the management objectives in this rule because this project will prevent shoaling in the boat basin and moorings and will perpetuate social, recreational, and economic values.

3. Section .0207 Public Trust Areas

The proposed project will not be detrimental to the public trust rights and the biological and physical functions of the estuary or open coastal waters. Interference with the use of the channel will be minimized during dredging activities. Dredges will be shifted or moved or dredging operations will be interrupted to accommodate the movement of vessels and floating equipment, if necessary. All spoils will be deposited and dewatered on the barges. The dredging and dewatering operations will be conducted within an impermeable floating turbidity curtain. The proposed project is consistent, to the maximum extent practicable, with the management objectives of public trust areas.

4. Section .0208 Use Standards

The proposed project is in conformance with the general and specific use standards of this section, and is therefore consistent with this management objective.

5. Section .0209 Coastal Shorelines

The project site is located adjacent to the estuarine shoreline and entirely within the Pasquotank River, as shown in Enclosure (3) Figure (2). There are no High Quality Water/Outstanding Resource Water Management Zones at the proposed project site as shown in Enclosure (3) Figure (4). Furthermore, the proposed project does not involve development or construction within the coastal shoreline. The proposed project will not have an effect on estuarine and public trust shorelines, and is therefore consistent with the management objectives of coastal shorelines.

BASIS OF DETERMINATION

6. Section .0300 Ocean Hazard Areas

The proposed project is not located along the Atlantic Ocean shoreline and contains no ocean hazard areas. Therefore, the proposed project is consistent with this management objective.

7. Section .0400 Public Water Supplies

The proposed project site is located in the Pasquotank River Basin (sub basin 30-01-50). The portion of the Pasquotank River where the project site is located (Index #: 30-3-(12)) is classified as “SB”, which is described as “Primary Recreation, Salt Water”. The proposed project site is not located within a small surface water supply watershed or public water supply field. The proposed project will not have an effect on public water supplies, and is therefore consistent with this management objective.

8. Section .0505 Coastal Areas that Sustain Remnant Species

The location of the proposed project site may affect, but is not likely to adversely affect, threatened and endangered species associated with the estuarine habitat of the Pasquotank River. As shown on the NC OneMap Habitat Map in Enclosure (3) Figure (5), there are no significant natural heritage areas in the vicinity of the project site; however, the project site is identified in a natural heritage element occurrence area. Reviews of the NC OneMap Habitat Map; the North Carolina Division of Marine Fisheries (DMF) Anadromous Fish Spawning Areas (AFSA) map for the Albemarle Sound area (Enclosure (3) Figure (6)); the DMF Fishery Nursery Areas map (Enclosure (3) Figure (7)); the National Oceanic and Atmospheric Administration Environmental Sensitivity Map for the Elizabeth City Quadrangle; and the South Atlantic Fishery Management Council’s Essential Fish Habitat (EFH) Map (Enclosure (3) Figure (8)) indicate that the proposed project site is located within AFSA waters and in an area of EFH; however, the project site is not located in the vicinity of a designated submerged aquatic vegetation area, a primary nursery area, or an EFH-Habitat Area of Particular Concern.

A review of the USFWS federally-listed species present in Pasquotank County, the USFWS Critical Habitat Mapper, and the North Carolina Natural Heritage Program (NHP) listed species for the Elizabeth City Quadrangle along with their habitat requirements was performed. Several state and federally-listed rare, threatened and endangered species occur in Pasquotank County. According to the online resources there are three federally-listed species identified within Pasquotank County with potential habitat in the project area (Shortnose sturgeon, *Acipenser brevirostrum*; Atlantic sturgeon, *Acipenser oxyrinchus*; and West Indian manatee, *Trichechus manatus*). Additionally, one federally-listed proposed threatened species (Red knot; *Calidris canutus rufa*) was identified within Pasquotank County and one state-listed threatened species (Bald eagle; *Haliaeetus leucocephalus*) was identified within the Elizabeth City quadrangle.

Review of the online USFWS Information, Planning, and Conservation (IPaC) System indicates that only two of the endangered species (i.e., Shortnose sturgeon and Atlantic sturgeon) and the proposed threatened species (i.e., Red knot) should be considered in an effects analysis for the proposed project. No critical habitats exist within the proposed project area.

The USCG is in the process of preparing an Environmental Assessment (EA) for the project, and pursuant to the requirements of the National Environmental Policy Act (NEPA), the USCG will be consulting with the USFWS, NHP, DMF, the North Carolina Wildlife Resources Commission (WRC), and the National Marine Fisheries Service regarding the presence of natural resources that may be potentially affected by the proposed project.

BASIS OF DETERMINATION

Measures will be taken to avoid or minimize adverse impacts to natural and biological resources. The proposed dredging project will only temporarily impact open waters (an area totaling approximately 0.18 acres) and will occur entirely within the existing boat basin (limited to an area approximately 150 feet from the shoreline). The dredging will be performed from a floating barge and not from land based operations. Prior to commencing dredging operations, the proposed area for dredging will be closed with an impermeable floating turbidity curtain. The turbidity curtain will remain in place in accordance with the permit conditions or for a period of 48 hours following completion of the dredging, whichever occurs longer. The dredge material will be dewatered on the barges within the turbidity curtain, transported by barge to an unloading site, offloaded, and transported by truck to an approved landfill. The dredging will be limited to the area indicated on the Dredge Plan presented in Enclosure (4).

Based on the project size and location and nature of the proposed work, the USCG does not anticipate significant adverse impacts to federally-listed species. To avoid potentially adverse impacts on life cycles of anadromous fish, the proposed project is expected to occur outside of the anadromous fish spawning moratorium (February 15 to June 30). Therefore, the USCG concludes that the proposed project may affect, but is not likely to adversely affect, any federally-listed threatened or endangered species or their habitats. Additionally, since the proposed dredging will occur entirely within the previously disturbed footprint of the boat basin and moorings and the dredge depth is expected to be proximate to the original dredge depth, the project is not expected to have an adverse effect on any unique habitat conditions that would impact the continued survival of threatened and endangered native plants and animals. The proposed project is consistent, to the maximum extent practicable, with this management objective.

9. Section .0506 Coastal Complex Natural Areas

The proposed project should not have an effect on:

- a. Natural conditions or the sites that function as key or unique components of coastal systems.
- b. The interactions of various life forms including sites that are necessary for the completion of life cycles, areas that function as links to other wildlife areas (wildlife corridors), and localities where the links between biological and physical environments are most fragile.
- c. Designated coastal complex natural areas.

As shown on the NC OneMap Habitat Map in Enclosure (3) Figure (5), the project site is not located in a designated submerged aquatic vegetation area or a significant natural heritage area. According to the habitat map, the project site is identified in a shellfish growing area. However, review of the NC DMF Shellfish Harvesting Area Closure Map (Enclosure (3) Figure (9)) indicates that the project site is located in a portion of the Pasquotank River where shellfish harvesting is prohibited (i.e., permanently closed). Additionally, there are no state parks, state natural areas, or state recreation areas at the project site. There are no national wildlife refuges, forests, historic properties, historic landmarks, national natural landmarks, no wild and scenic rivers, or other fragile areas at the project location.

The proposed project site is located within a portion of the Pasquotank River designated as an AFSA (Conservation Planning Tool Rating of 8). To avoid potentially adverse impacts on life cycles of anadromous fish, the proposed project is expected to occur outside of the anadromous

BASIS OF DETERMINATION

fish spawning moratorium (February 15 to June 30). To avoid potentially adverse impacts to water quality, the proposed area for dredging will be closed with an impermeable floating turbidity curtain with a weighted bottom edge. The dredge material will be dewatered on the barges within the turbidity curtain and the turbidity curtain will remain in place in accordance with the permit conditions or for a period of 48 hours following completion of the dredging, whichever occurs longer. Therefore, the proposed project is not expected to have an adverse effect on designated coastal complex natural areas and is consistent, to the maximum extent practicable, with this management objective.

10. Section .0507 Unique Coastal Geologic Formations

The proposed project will not have an effect on any unique geologic formations, and is therefore consistent with this management objective.

11. Section .0509 Significant Coastal Archaeological Resources

The USCG has determined that there are no National Register-listed or –eligible, or potentially eligible (according to the North Carolina State Historic Preservation Office [NC SHPO] Study List) terrestrial or underwater archaeological sites located within or immediately adjacent to the proposed project site. No previously identified shipwrecks or other underwater archaeological resources have been recorded in the proposed project site area.

The proposed dredging activities will not create any subsurface disturbance that could affect terrestrial archaeological resources, if present. Dredging will not be staged on the shore, and all dredged sediment will be loaded onto a barge. Although maintenance dredging has not been previously conducted for the boat basin and moorings, the actions required for the original construction of the two-lane concrete ramp, launch dock, wooden piers, boat basin, and associated moorings have undoubtedly disturbed the sediments to a considerable depth. Due to the extent of prior subsurface disturbance in the proposed project site, it is unlikely that intact, significant underwater archaeological resources would be present and retain integrity. Therefore, the USCG has determined “No Historic Properties Affected.” The USCG consulted with the NC SHPO regarding the determination and received concurrence from the NC SHPO which includes comments from the Office of State Archaeology.

The proposed project will not have an effect on any significant coastal archaeological resources, and is therefore consistent with this management objective.

12. Section .0510 Significant Coastal Historic Architectural Resources

The USCG has determined that there are no National Register-listed or –eligible, or potentially eligible (according to the NC SHPO Study List) historic architectural resources located within or immediately adjacent to the proposed project site. Three historic architectural resources have been identified within one mile of the proposed project site, including one site that has been Determined Eligible and two potentially eligible sites on the SHPO Study List. The National Register-eligible site was photo documented and demolished in 2013 upon the SHPO’s approval. The two potentially eligible sites on the SHPO Study List are located inland, approximately one mile from the project site. Therefore, the USCG has determined “No Historic Properties Affected.” The USCG consulted with the NC SHPO regarding the determination and received concurrence from the NC SHPO.

The proposed project will not have an effect on any significant coastal historic architectural resources, and is therefore consistent with the management objective.

BASIS OF DETERMINATION***13. Section .0600 - Development Standards Applicable to All AECs***

The proposed project will not contravene or violate any Federal, State of North Carolina, and local rules, regulations, laws, or requirements. The proposed project will not cause pollution to the state shellfish waters, and will not have an effect on airspace activity or noise pollution associated with airspace activity. Therefore, the proposed project is consistent with the management objectives of development standards applicable to all AECs.

Subchapter 7M - General Policy Guidelines for the Coastal Area**Policies for Projects Outside Areas of Environmental Concern*****1. Section .0200 - Shoreline Erosion Policies***

The proposed project will not have an adverse impact to shoreline erosion in the area, and is therefore consistent with the shoreline erosion policies.

2. Section .0300 - Shorefront Access Policies

The USCG Base Elizabeth City facility is a fenced and gated Federal facility that restricts public access. Since public access is already restricted, the proposed project will not have an adverse impact on the accessibility of the facility's shorefront to the public. Therefore, the proposed project is consistent with the shorefront access policies.

3. Section .0400 - Coastal Energy Policies

The USCG facility and the proposed project are not associated with the development of energy facilities and energy resources. Therefore, the proposed project is consistent with the coastal energy policies.

4. Section .0500 - Post-Disaster Policies

The proposed project does not involve construction; therefore, pre-disaster planning is not required. This proposed project is therefore consistent with the post-disaster policies.

5. Section .0600 - Floating Structure Policies

The proposed project will involve the use of a floating plant and barges; however, the floating plant and barges are not considered "floating structures" as defined in Section .0602 since they will not be inhabited or used for commercial purposes for more than 30 days. Therefore, the proposed project is consistent with the floating structure policies.

6. Section .0700 - Mitigation Policy

The proposed project will not have an adverse impact to coastal lands or waters which would require mitigation, and is therefore consistent with the mitigation policy.

7. Section .0800 - Coastal Water Quality Policies

The proposed project will not cause the degradation of water quality so as to impair traditional uses of the coastal waters. No hazardous materials or petroleum products are expected to be discharged at the project location. The proposed project does involve in-water dredging that will generate waste sediments; however, the dredge sediment material has been characterized as a non-hazardous waste and has been approved for offsite disposal at a licensed and approved Subtitle D landfill permitted to accept Special Waste (East Carolina Regional Landfill). Therefore, this proposed project is consistent with the coastal water quality policies.

BASIS OF DETERMINATION

8. Section .0900 - Policies on Use of Coastal Airspace

The proposed project does not involve aviation or coastal airspace resources, and is therefore consistent with the coastal airspace policies.

9. Section .1000 - Policies on Water and Wetland Based Target Areas for Military Training Activities

The proposed project is not involved with military training activities, and is therefore consistent with this coastal policy.

10. Section .1100 - Policies on Beneficial Use and Availability of Materials Resulting from the Excavation or Maintenance of Navigational Channels

The proposed project includes in-water dredging that will generate sediments requiring disposal; however, the material will not be dredged from the navigation channel. Furthermore, previous sediment sampling indicates low concentrations of metals and polynuclear aromatic hydrocarbons may be present. Therefore, the proposed project is consistent with this coastal policy.

11. Section .1200 - Policies on Ocean Mining

The proposed project does not include any mining activities, and is therefore consistent with the ocean mining policies.

Conclusion

The USCG conducted a coastal zone management Federal consistency review of the proposed project and determined that the proposed project will not have any significant impacts to the coastal resources. Therefore, the proposed project is consistent, to the maximum extent practicable, with the enforceable policies of North Carolina's federally approved coastal management program.

ENCLOSURE (3)

FIGURES 1 – 9

FIGURE (1): SITE LOCATION MAP

FIGURE (2): COASTAL WETLANDS MAP

FIGURE (3): USFWS WETLANDS MAP

FIGURE (4): WATER QUALITY MAP

FIGURE (5): NATURAL HERITAGE AREAS/HABITAT MAP

FIGURE (6): NCDMF ANADROMOUS FISH SPAWNING AREAS MAP

FIGURE (7): NCDMF FISHERY NURSERY AREAS MAP

FIGURE (8): SAFMC ESSENTIAL FISH HABITAT MAP

FIGURE (9): NCDMF SHELLFISH HARVESTING CLOSURE MAP



**Figure (1): Topographic Map – 1983
Elizabeth City, NC Topographic Quadrangle**

Enclosure (3)



Figure (2): North Carolina Coastal Wetlands Map

Enclosure (3)

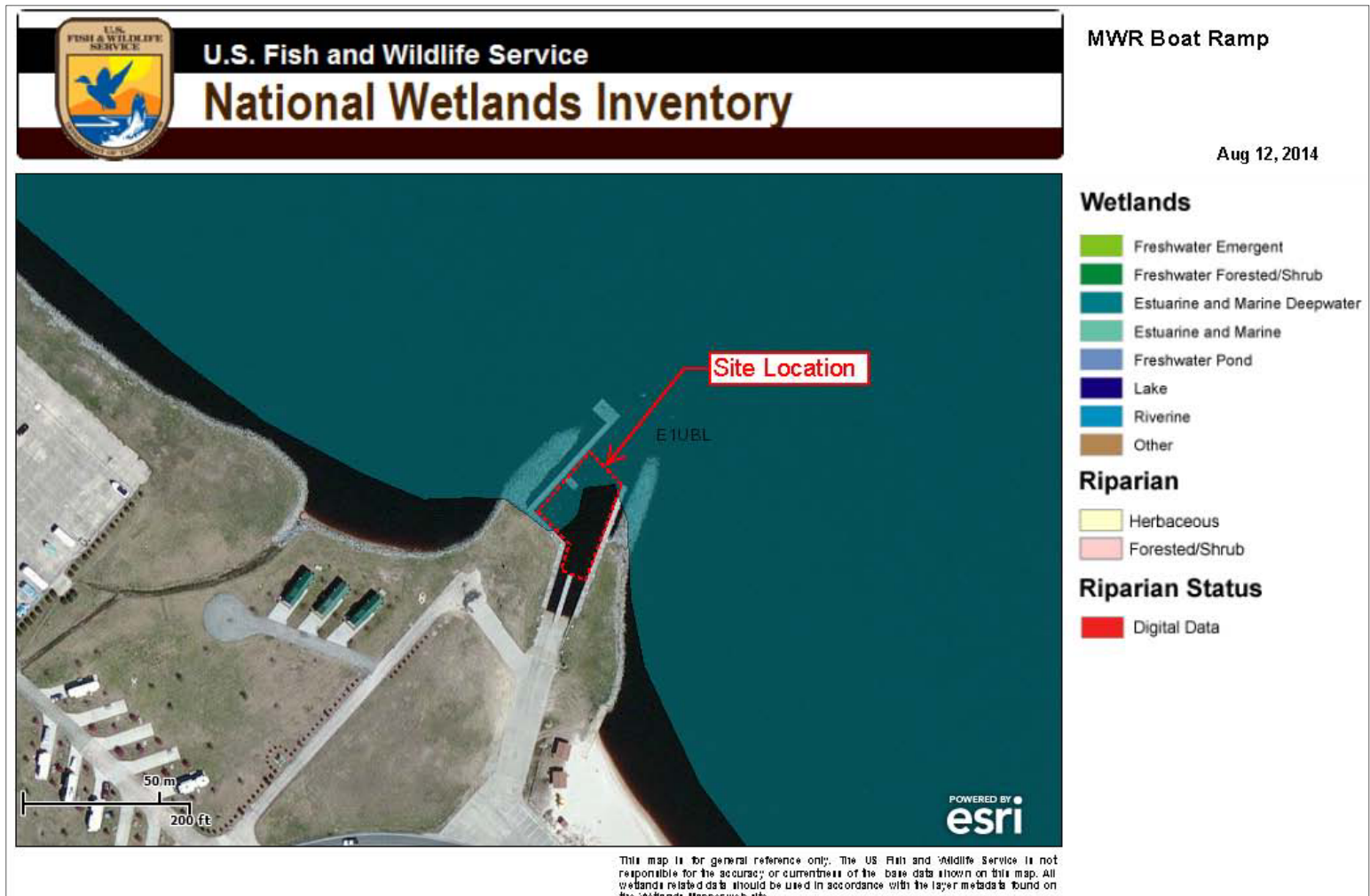


Figure (3): USFWS Wetlands Map

Enclosure (3)

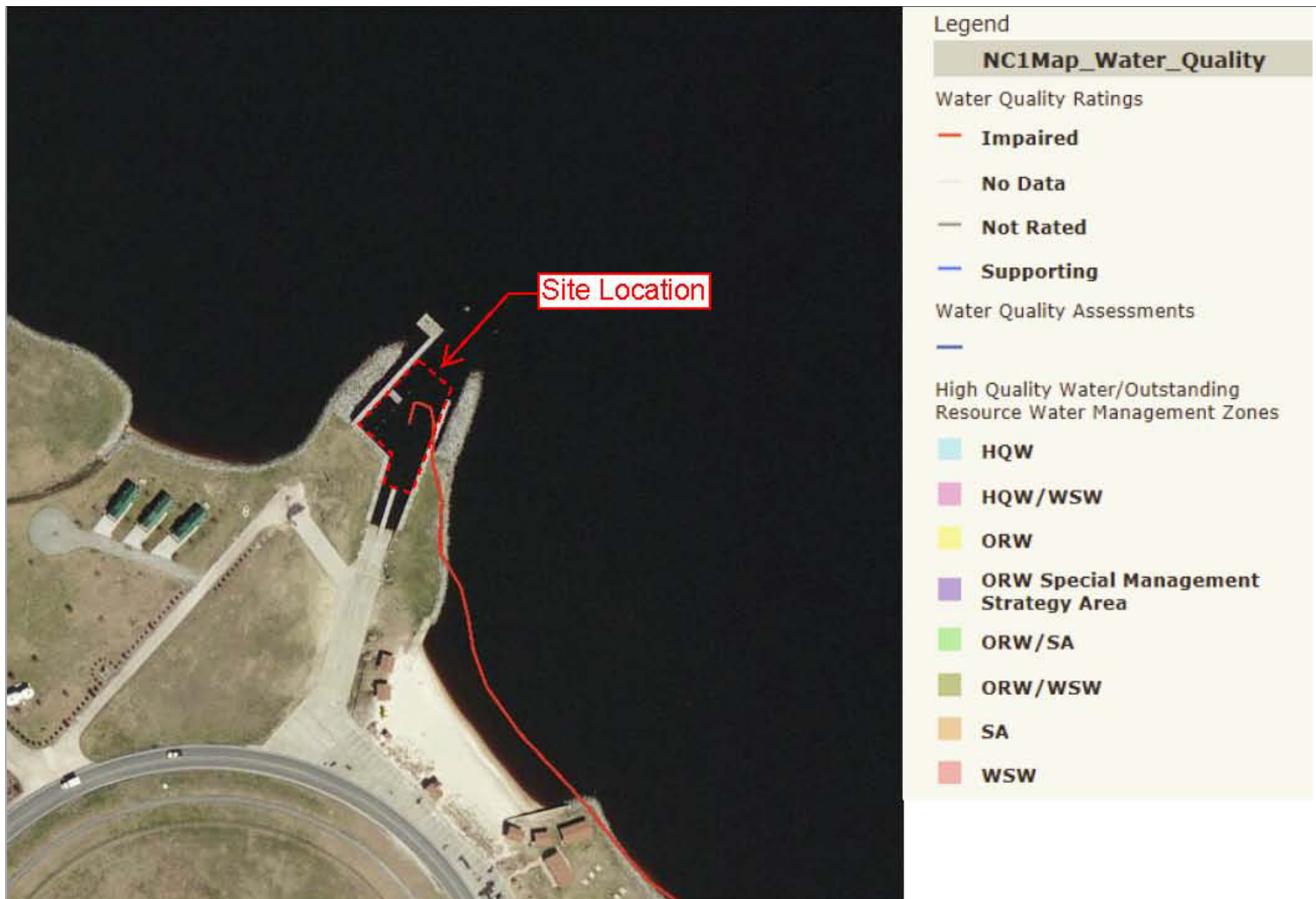


Figure (4): NC OneMap Water Quality Map

Enclosure (3)

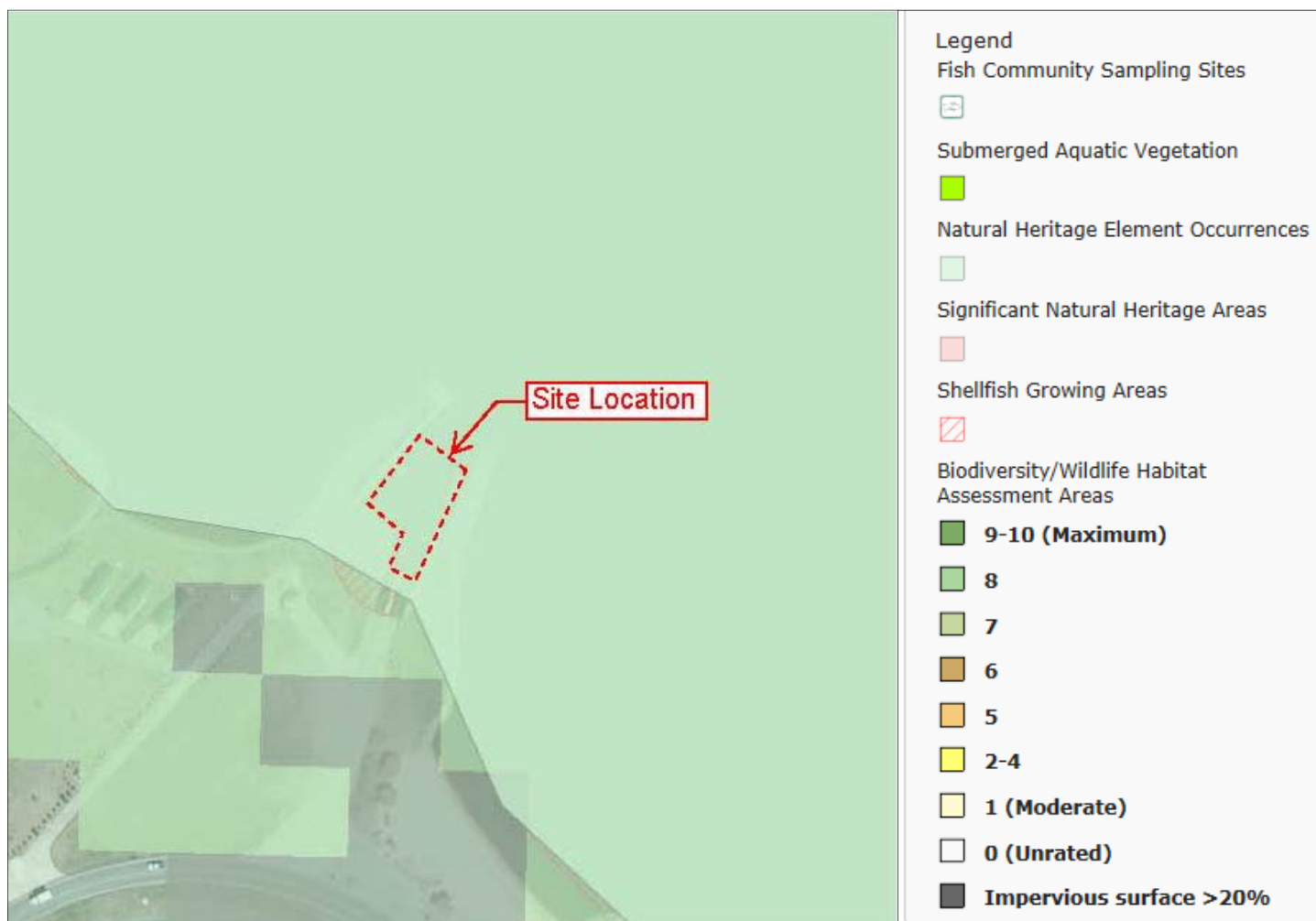


Figure (5): NC OneMap Habitat Map

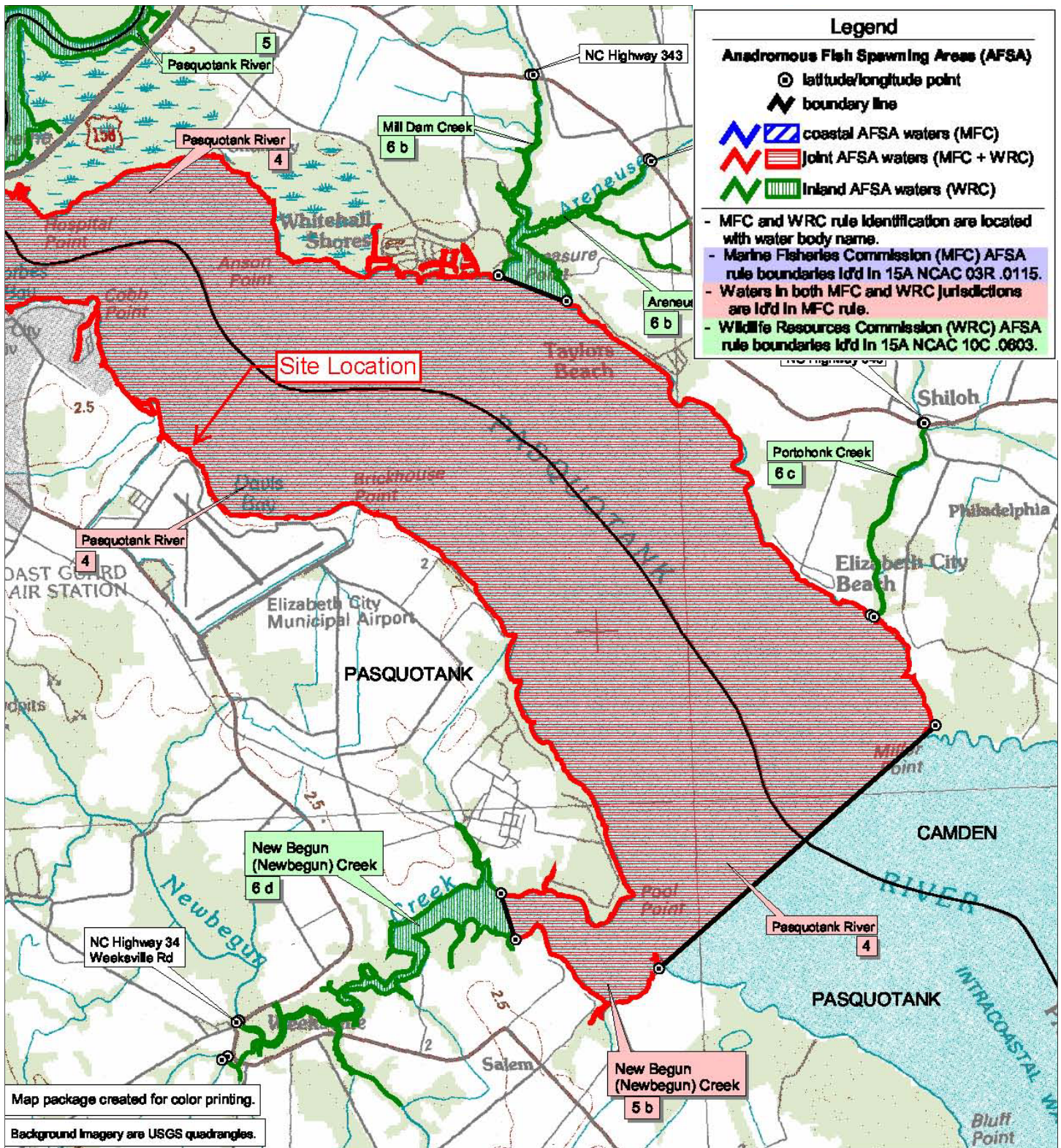


Figure (6): NC DMF Anadromous Fish Spawning Areas Map

Enclosure (3)

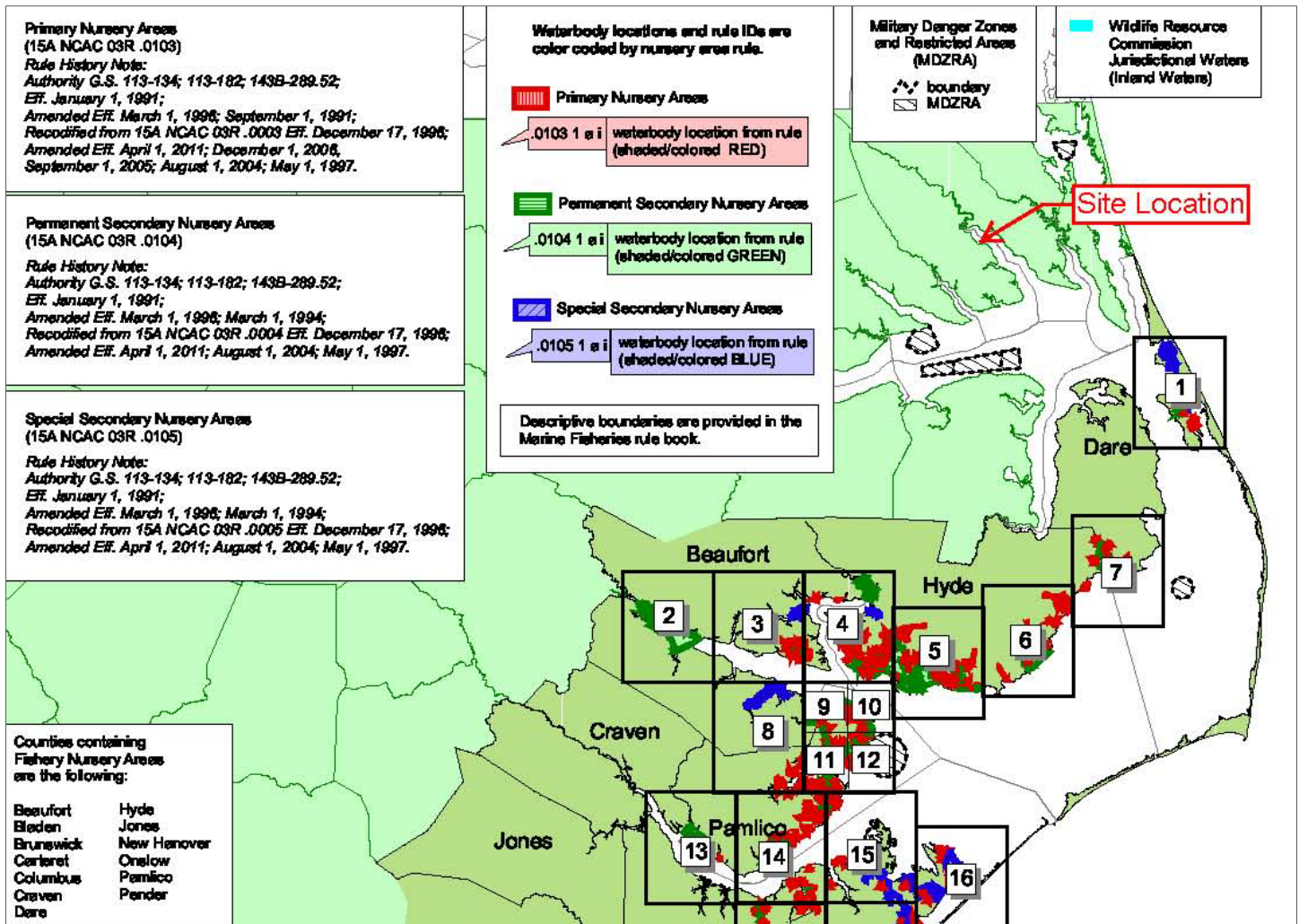


Figure (7): NC DMF Fishery Nursery Areas Map

Enclosure (3)

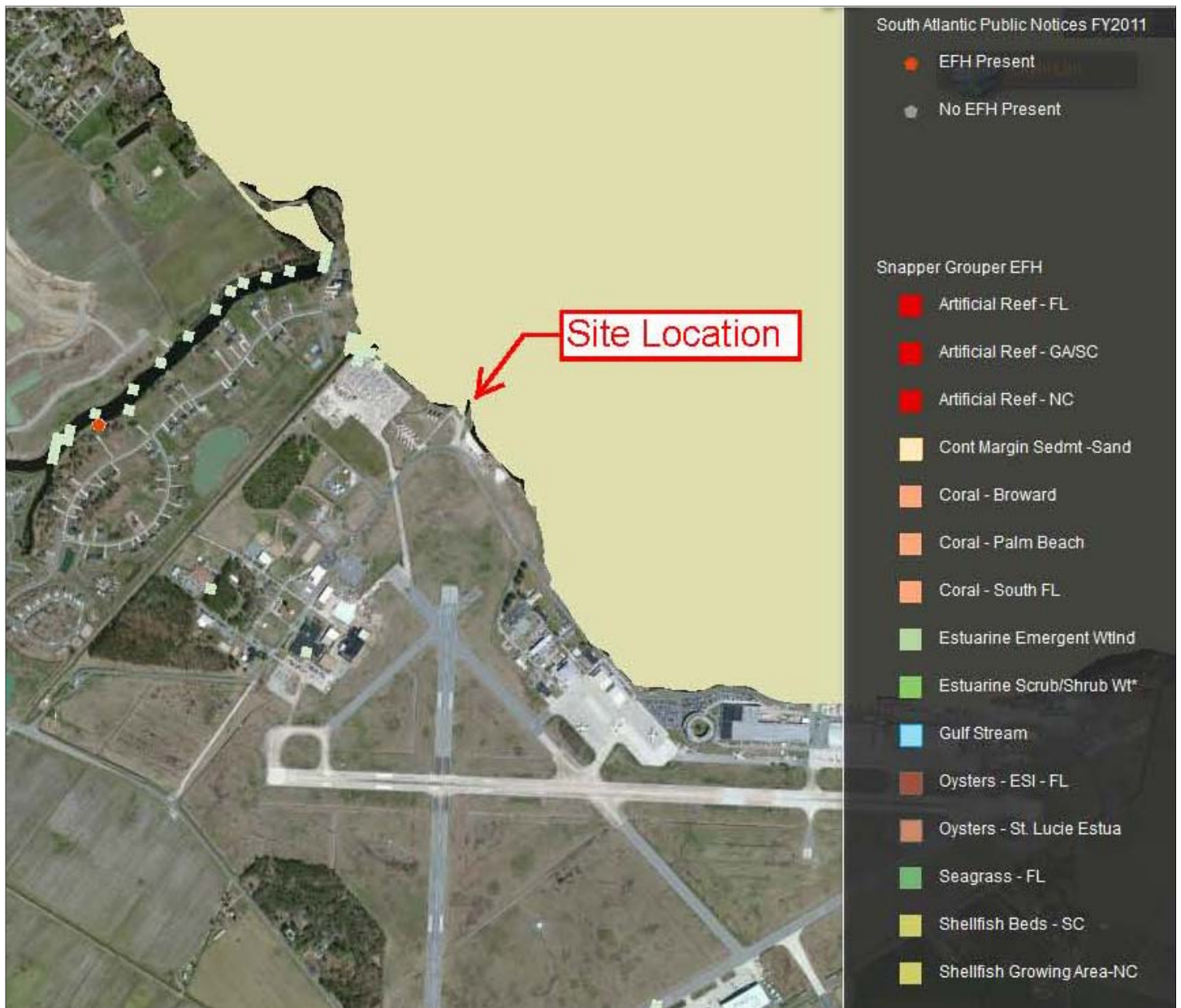


Figure (8): SAFMC Essential Fish Habitat Map

Enclosure (3)

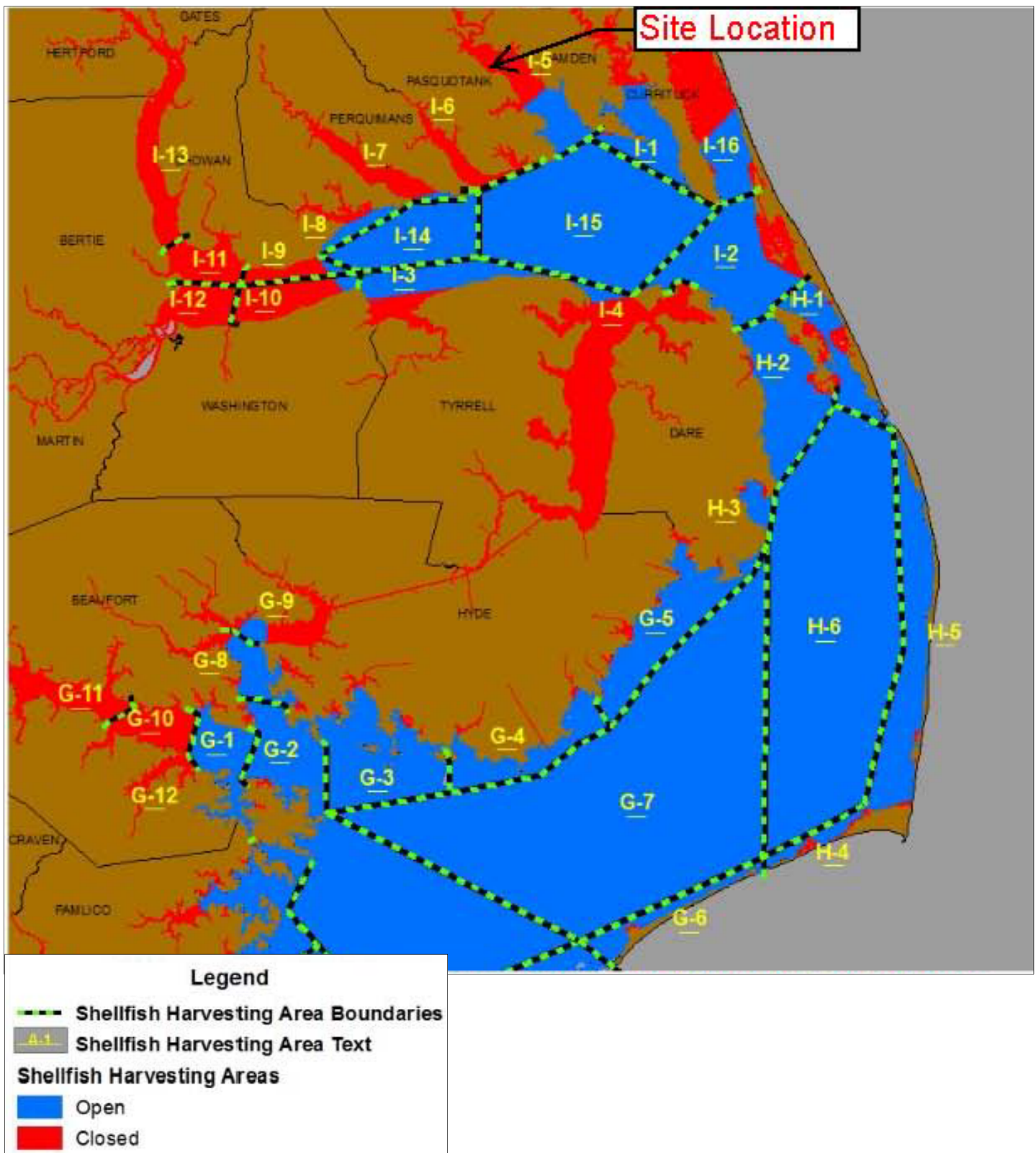


Figure (9): NC DMF Shellfish Harvesting Area Closure Map

ENCLOSURE (4)

DREDGE PLAN – EXISTING FEATURES

FIGURE (10): DREDGE PLAN

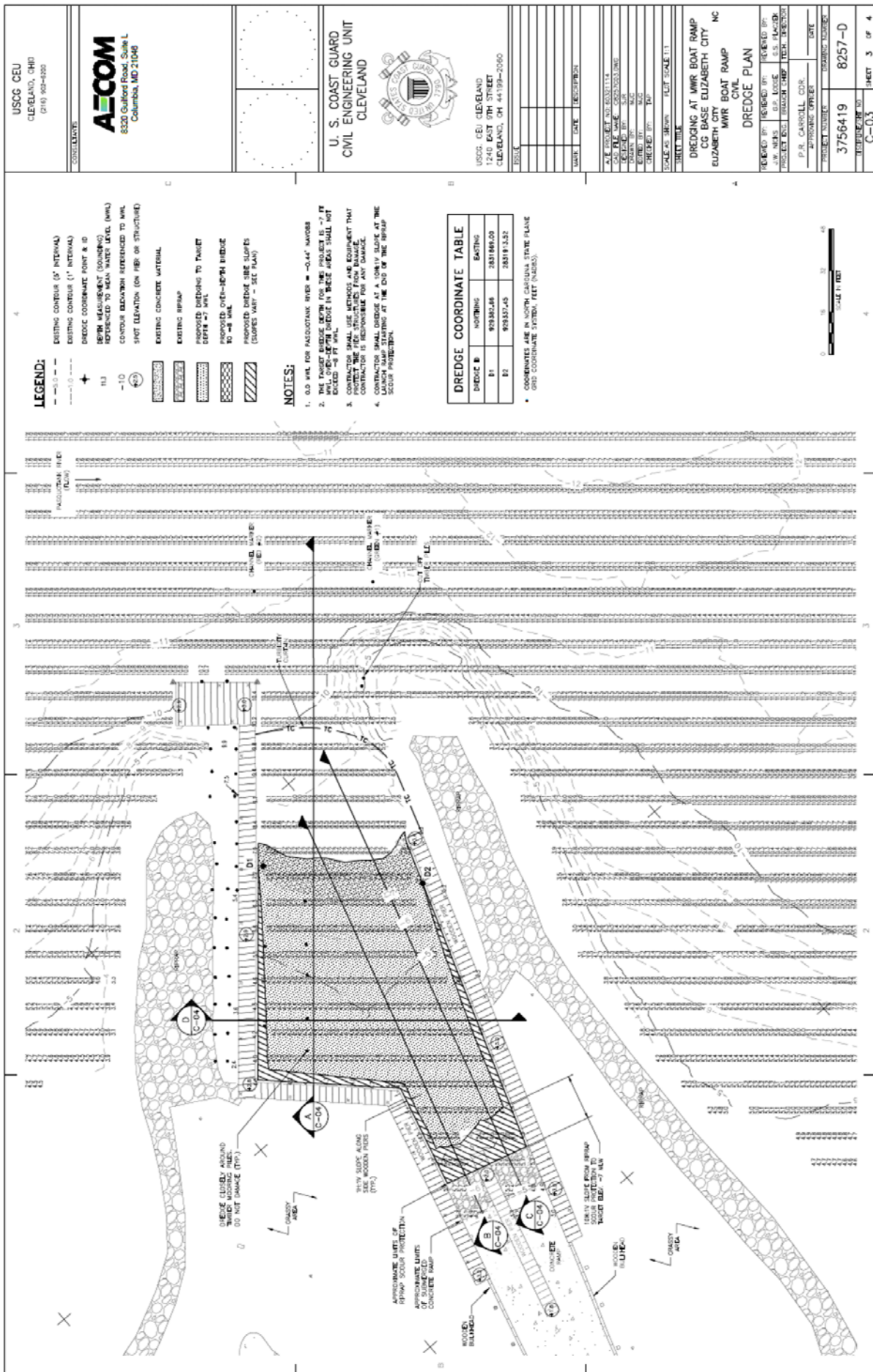


Figure 10: Dredge Plan



North Carolina Department of Environment and Natural Resources
Division of Coastal Management

Pat McCrory
Governor

Donald R. van der Vaart
Secretary

March 19, 2015

Gregory O. Carpenter
Chief, Environmental Compliance
United States Coast Guard
1240 East Ninth Street
Room 2179
Cleveland Ohio, 44199-2060

SUBJECT: **CD15-004**— Consistency Concurrence Concerning the proposed maintenance dredging at the Morale, Welfare and Recreate Boat Ramp at the USCG Base Elizabeth City, Pasquotank County, North Carolina (DCM#20150003)

Dear Mr. Carpenter,

We received your consistency submission on January 13, 2015 concerning proposed maintenance dredging at the Morale, Welfare and Recreate Boat Ramp at the USCG Base Elizabeth City, Pasquotank County, North Carolina.

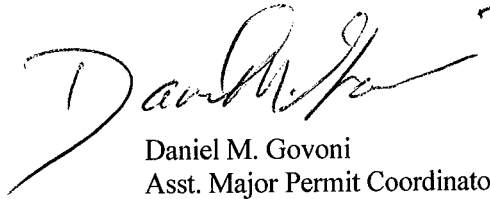
North Carolina's coastal zone management program consists of, but is not limited to, the Coastal Area Management Act, the State's Dredge and Fill Law, Chapter 7 of Title 15A of North Carolina's Administrative Code, and the land use plan of the County and/or local municipality in which the proposed project is located. It is the objective of the Division of Coastal Management (DCM) to manage the State's coastal resources to ensure that proposed Federal activities would be compatible with safeguarding and perpetuating the biological, social, economic, and aesthetic values of the State's coastal waters.

To solicit public comments, DCM circulated a description of the proposed project to State agencies that would have a regulatory interest. No comments asserting that the proposed activity would be inconsistent with the State's coastal management program were received.

DCM has reviewed the submitted information pursuant to the management objectives and enforceable policies of Subchapters 7H and 7M of Chapter 7 in Title 15A of the North Carolina Administrative Code and concurs that the proposed Federal activity by the US Coast Guard is consistent, to the maximum extent practicable, with North Carolina's certified coastal management program.

Should the proposed action be modified, a revised consistency determination could be necessary. This might take the form of either a supplemental consistency determination pursuant to 15 CFR 930.46, or a new consistency determination pursuant to 15 CFR 930.36. Likewise, if further project assessments reveal environmental effects not previously considered by the proposed development, a supplemental consistency certification may be required. If you have any questions, please contact Daniel Govoni at 252-808-2808 x215. Thank you for your consideration of the North Carolina Coastal Management Program.

Sincerely,

A handwritten signature in black ink, appearing to read "Daniel M. Govoni", with a large, stylized initial "D" on the left.

Daniel M. Govoni
Asst. Major Permit Coordinator

Appendix D.

Consultations with State and Federal Agencies



AECOM
701 Corporate Center Drive, Suite 475
Raleigh, NC 27607

919.854.6200 tel
919.854.6259 fax

October 30, 2014

Ms. Allison Weakley
NC Office of Land and Water Stewardship
NC Natural Heritage Program
1601 Mail Service Center
Raleigh, NC 27699-1601

Subject: U.S. Coast Guard Base Elizabeth City – Dredging at MWR Boat Ramp, Elizabeth City, North Carolina

Dear Ms. Weakley:

The U.S. Coast Guard (USCG) Civil Engineering Unit Cleveland is in the process of preparing an Environmental Assessment (EA) for proposed maintenance dredging of the boat basin and moorings at the Morale, Welfare and Recreate (MWR) Boat Ramp at the USCG Base Elizabeth City. The USCG Base Elizabeth City is located approximately 3.5 miles southeast of Elizabeth City, along Highway 34, on the south shore of the Pasquotank River in Pasquotank County, North Carolina (**Attachment A**). The geographic location of the MWR Boat Ramp is latitude 36° 16' 14.72" north and longitude 76° 10' 40.31" west.

The MWR Boat Ramp is a previously dug man-made basin and the proposed dredging will occur entirely within the previously disturbed footprint of the boat basin and moorings. However, the USCG has no record of the as-built drawings indicating the original dredge depth. While the dredging required for the original construction of the MWR Boat Ramp likely disturbed the sediments to a depth proximate to the proposed dredge depth, the exact depth of the original dredging is not documented. Therefore, the USCG has elected to conduct an EA to address National Environmental Policy Act of 1969 requirements. The EA will determine whether or not there is potential for significant impact to the human and natural environments. Following completion of the EA, the USCG will either file a Finding of No Significant Impact or prepare an Environmental Impact Statement. This letter provides the North Carolina Natural Heritage Program (NC NHP) with information on the boat ramp construction, existing environment, and proposed action in advance of issuance of the EA for this project. The USCG would appreciate any comments the NC NHP may have for consideration in preparation of the EA.

The MWR Boat Ramp is located on the north end of the Base's shoreline, in a portion of the river tidal flats. The MWR Boat Ramp consists of a two-lane concrete ramp with a concrete pavement approach, and a launch dock centered in the boat ramp (**Attachments B and C**). Rubble mound stone breakwaters project approximately 100 to 125 feet from the shoreline on both sides of the launch ramp to form a protected harbor area for launching and retrieving boats. Timber docks line both sides of the boat basin. Mooring piles are in place along the west timber dock. Navigation aids are located near the entrance to the boat basin. These consist of a red dayboard with a solar-powered red beacon, and a green dayboard. The MWR Boat Ramp is utilized by base military personnel for launching privately-owned recreational boats, and for launching USCG vessels stationed at the Building 43 moorings.

The purpose of this project is to dredge the boat basin and moorings to a required depth below mean water level at the MWR Boat Ramp due to changing conditions at the site. Water depth in the boat basin and moorings is reduced by heavy winds from the north, which push water from the boat basin and down the Pasquotank River. This wind effect decreases the depth of the water and

encumbers the USCG's ability to utilize the existing boat ramp for both operational missions and recreational purposes. The project involves mechanical dredging of the boat basin and moorings to a maximum depth of 8 feet below mean water level (includes 1 foot over dredge depth). Mean water level corresponds to elevation -0.44 North American Vertical Datum 1988 (NAVD88). Therefore, dredging will proceed to -8.44 feet NAVD88. The calculated dredge volume for the MWR Boat Ramp is 755 cubic yards of sediment. The proposed dredging project will only temporarily impact open waters (an area totaling approximately 0.18 acres) and will occur entirely within the existing boat basin (limited to an area approximately 150 feet from the shoreline). The 35 percent civil site plan entitled, "Dredging at MWR Boat Ramp: CG Base Elizabeth City" illustrates the location and specifications of this proposed dredging project (**Attachment D**).

During the proposed dredging activities, the dredge area will be closed with an impermeable floating turbidity curtain with a weighted bottom edge. The dredge material will be deposited on barges, dewatered on the barges, transported by barge to an unloading site, offloaded to trucks, and transported by truck to the East Carolina Regional Landfill in Aulander, North Carolina for disposal. Formal approval for disposal of the dredge material at the MWR Boat Ramp has been received from the East Carolina Regional Landfill.

The USCG has reviewed the U.S. Fish and Wildlife Service (USFWS) Threatened and Endangered Species and Species of Concern for Pasquotank County and the NC NHP listed species for the Elizabeth City Quadrangle along with their habitat requirements. Table 1 is a list of rare, threatened and endangered species documented to occur in Pasquotank County and the Elizabeth City quadrangle where the project site is located (**Attachment E**). The USCG also reviewed the USFWS Critical Habitat Mapper and the online USFWS Information, Planning, and Conservation (IPaC) System.

According to the online resources there is one state-listed endangered species identified within the Elizabeth City quadrangle (Shortnose sturgeon; *Acipenser brevirostrum*) with potential habitat in the project area and two additional federally-listed endangered species (Atlantic sturgeon; *Acipenser oxyrinchus* and West Indian manatee; *Trichechus manatus*) identified within Pasquotank County with potential habitat in the project area. One state-listed threatened species (Bald eagle; *Haliaeetus leucocephalus*) was also identified within the Elizabeth City quadrangle with potential habitat in the project area. Additionally, one federally-listed proposed threatened species (Red knot; *Calidris canutus rufa*) was identified within Pasquotank County. Review of the IPaC System indicates that only two of the endangered species (i.e., Shortnose sturgeon and Atlantic sturgeon) and the proposed threatened species (i.e., Red knot) should be considered in an effects analysis for the proposed project. No critical habitats were identified within the proposed project area.

The USCG requests that the NC NHP provide any additional information or potential concerns regarding the presence of threatened and endangered species or other significant natural resources that may be potentially affected by the maintenance dredging of the boat basin and moorings at the MWR Boat Ramp. Any comments or recommendations that your agency may have for avoiding potential impacts associated with this project would also be appreciated. We would appreciate receipt of your written approval within 30 days of this letter; this will help us maintain the overall schedule for this project.

If you have any questions, please contact Jennifer Jones (AECOM) at (919) 239-7150 or Leilani Woods (USCG) at (252) 335-6847.

Yours sincerely,



Jennifer Jones
Environmental Task Manager



Ron Johnson
Senior Biologist

Cc: Ms. Leilani Woods, Environmental Engineer, USCG Civil Engineering Unit Cleveland
Mr. Greg Carpenter, Environmental Project Manager, USCG Civil Engineering Unit Cleveland

Attachments: (A) Dredge Site - Location Map
(B) Dredge Site - Aerial Photograph
(C) Dredge Site - Ground-Level Photographs
(D) Dredge Site – 35% Plan
(E) Table 1 – Listing of Rare, Threatened and Endangered Species
(F) References Cited

ATTACHMENT (A)

DREDGE SITE - LOCATION MAP



Map Location



Site Location Map

MWR Boat Ramp
USCG Base Elizabeth City, North Carolina
Elizabeth City NC Topographic Quadrangle (1983)

June 2014

0 1,000 2,000
Feet

60321114

Figure 1

AECOM

AECOM Environment
701 Corporate Center Drive, Suite 475
Raleigh, NC 27607
Phone: (919) 854-6200
Fax: (919) 854-6259
Web: <http://www.aecom.com>

ATTACHMENT (B)

DREDGE SITE – AERIAL PHOTOGRAPH



Scale: Not To Scale

(Source: Google Earth© 2014)

Dredge Site - Aerial Photograph

US Coast Guard
Base Elizabeth City – MWR Boat Ramp
Elizabeth City, NC

AECOM

Attachment B

www.aecom.com

ATTACHMENT (C)

DREDGE SITE – GROUND-LEVEL PHOTOGRAPHS

Facility Name: United States Coast Guard	Site Location: MWR Boat Ramp – USCG Base Elizabeth City, NC	Project No. 60321114
--	---	--------------------------------

Photo No. 1	Date: 05/7/14	
Direction Photo Taken: North		
Description: View from the southern side of the shore (concrete pavement approach) of the concrete ramp, docks, navigation aids, and light pedestals.		

Photo No. 2	Date: 05/7/14	
Direction Photo Taken: Northwest		
Description: View from the concrete pavement approach along the southern shoreline.		

Facility Name: United States Coast Guard	Site Location: MWR Boat Ramp – USCG Base Elizabeth City, NC	Project No. 60321114
--	---	--------------------------------

Photo No. 3	Date: 05/7/14	
Direction Photo Taken: South		
Description: View looking south of the concrete ramp, docks, light pedestals, and mooring piles.		

Photo No. 4	Date: 05/7/14
Direction Photo Taken: North	
Description: View at the end of the concrete ramp of the docks, light pedestals, and mooring piles.	



Facility Name: United States Coast Guard	Site Location: MWR Boat Ramp – USCG Base Elizabeth City, NC	Project No. 60321114
--	---	--------------------------------

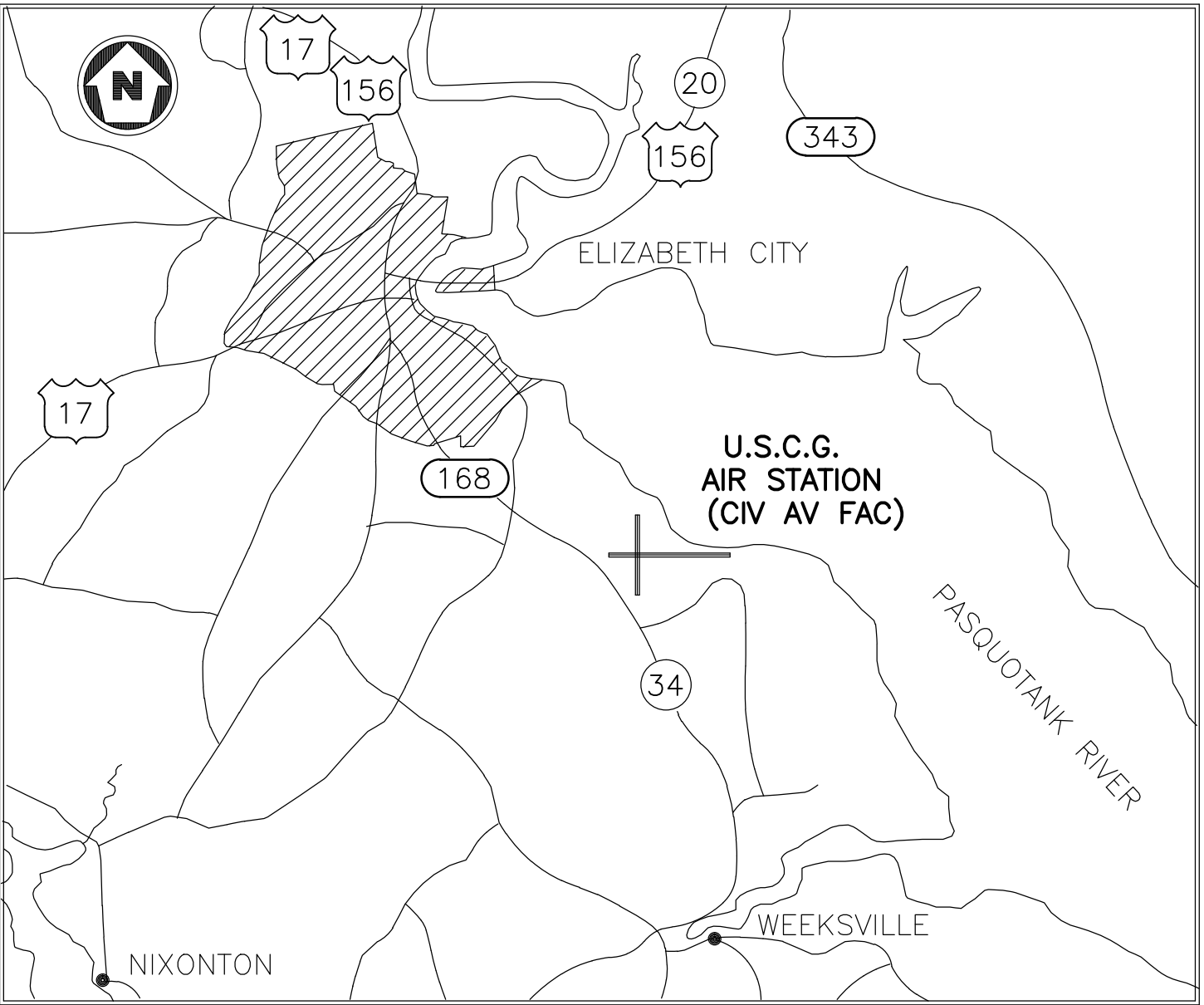
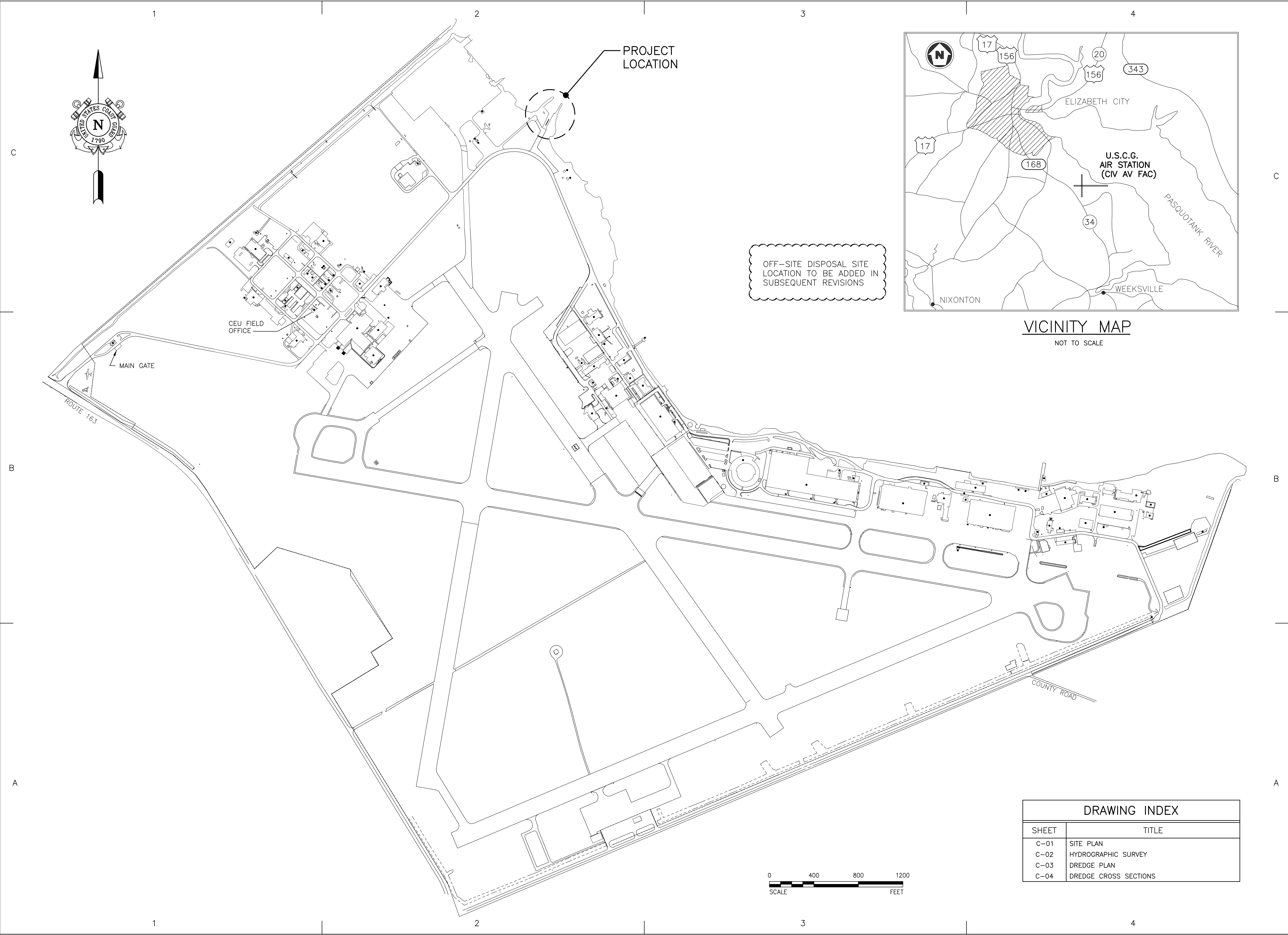
Photo No. 5	Date: 05/7/14
Direction Photo Taken: South-Southeast	
Description: View of the man-made beach located southeast of the MWR Boat Ramp.	



Photo No. 6	Date: 05/7/14	
Direction Photo Taken: North		
Description: View of the navigation aids at entry to the protected harbor area. Note two timbers low to water surface in the entry-way.		

ATTACHMENT (D)

DREDGE SITE – 35% PLAN



VICINITY MAP

NOT TO SCALE

DRAWING INDEX

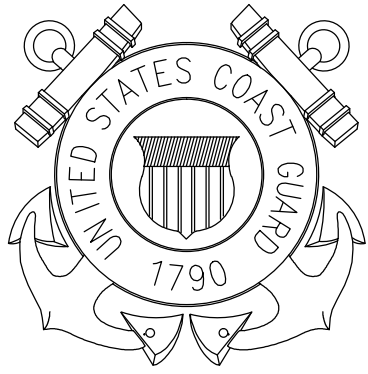
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C-01	SITE PLAN
C-02	HYDROGRAPHIC SURVEY
C-03	DREDGE PLAN
C-04	DREDGE CROSS SECTIONS

USCG CEU
CLEVELAND, OHIO
(216) 902-6200

CONSULTANTS

AECOM
8320 Guilford Road, Suite L
Columbia, MD 21046

U. S. COAST GUARD
CIVIL ENGINEERING UNIT
CLEVELAND



USCG. CEU CLEVELAND
1240 EAST 9TH STREET
CLEVELAND, OH 44199-2060

ISSUE		
MARK	DATE	DESCRIPTION

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DESIGNED BY:	SJR
DRAWN BY:	MJC
EDITED BY:	MJC
CHECKED BY:	TAP

SCALE: AS SHOWN PLOT SCALE: 1:1

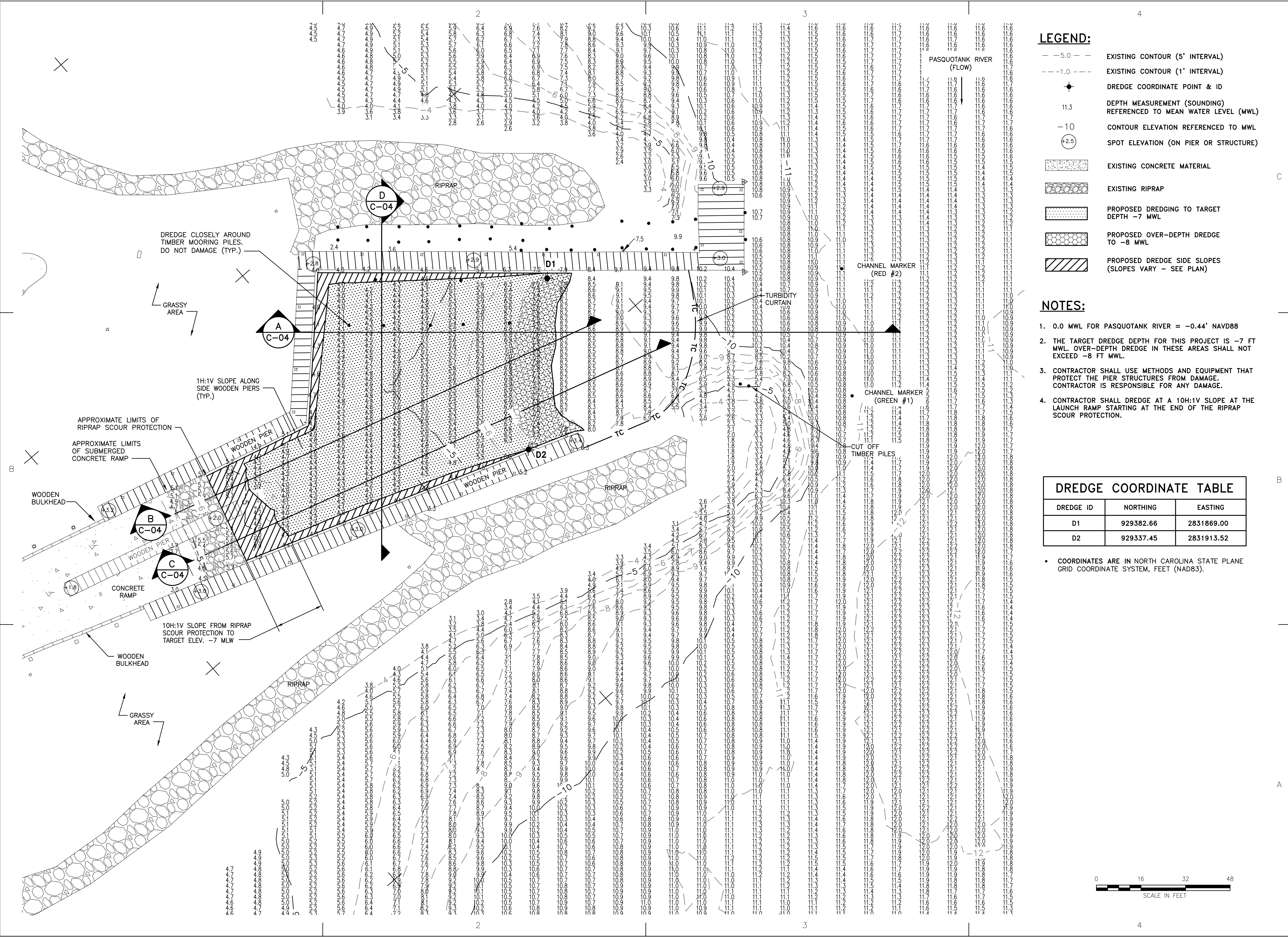
SHEET TITLE

DREDGING AT MWR BOAT RAMP
CG BASE ELIZABETH CITY
ELIZABETH CITY NC
MWR BOAT RAMP
CIVIL
SITE PLAN

REVIEWED BY:	REVIEWED BY:	REVIEWED BY:
J.W. NICKS	G.P. LODGE	G.S. PLACZEK
PROJECT ENG.	BRANCH CHIEF	TECH. DIRECTOR

P.R. CARROLL, CDR.	
APPROVING OFFICER	DATE

PROJECT NUMBER	DRAWING NUMBER
3756419	8257-D
DISCIPLINE/SHT NO	SHEET 1 OF 4
C-01	



LEGEND:

- 5.0--- EXISTING CONTOUR (5' INTERVAL)
- 1.0--- EXISTING CONTOUR (1' INTERVAL)
- DREDGE COORDINATE POINT & ID
- 11.3 DEPTH MEASUREMENT (SOUNDING) REFERENCED TO MEAN WATER LEVEL (MWL)
- 10 CONTOUR ELEVATION REFERENCED TO MWL
- ⊕2.5 SPOT ELEVATION (ON PIER OR STRUCTURE)
- [Pattern] EXISTING CONCRETE MATERIAL
- [Pattern] EXISTING RIPRAP
- [Pattern] PROPOSED DREDGING TO TARGET DEPTH -7 MWL
- [Pattern] PROPOSED OVER-DEPTH DREDGE TO -8 MWL
- [Pattern] PROPOSED DREDGE SIDE SLOPES (SLOPES VARY - SEE PLAN)

NOTES:

- 0.0 MWL FOR PASQUOTANK RIVER = -0.44' NAVD88
- THE TARGET DREDGE DEPTH FOR THIS PROJECT IS -7 FT MWL. OVER-DEPTH DREDGE IN THESE AREAS SHALL NOT EXCEED -8 FT MWL.
- CONTRACTOR SHALL USE METHODS AND EQUIPMENT THAT PROTECT THE PIER STRUCTURES FROM DAMAGE. CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE.
- CONTRACTOR SHALL DREDGE AT A 10H:1V SLOPE AT THE LAUNCH RAMP STARTING AT THE END OF THE RIPRAP SCOUR PROTECTION.

DREDGE COORDINATE TABLE		
DREDGE ID	NORTHING	EASTING
D1	929382.66	2831869.00
D2	929337.45	2831913.52

- COORDINATES ARE IN NORTH CAROLINA STATE PLANE GRID COORDINATE SYSTEM, FEET (NAD83).

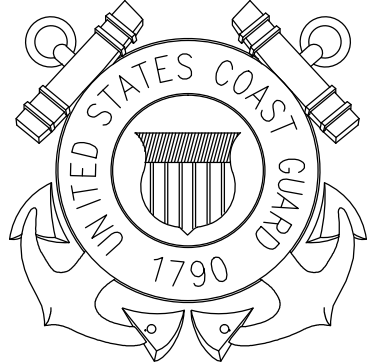


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(216) 902-6200

CONSULTANTS

AECOM
8320 Guilford Road, Suite L
Columbia, MD 21046

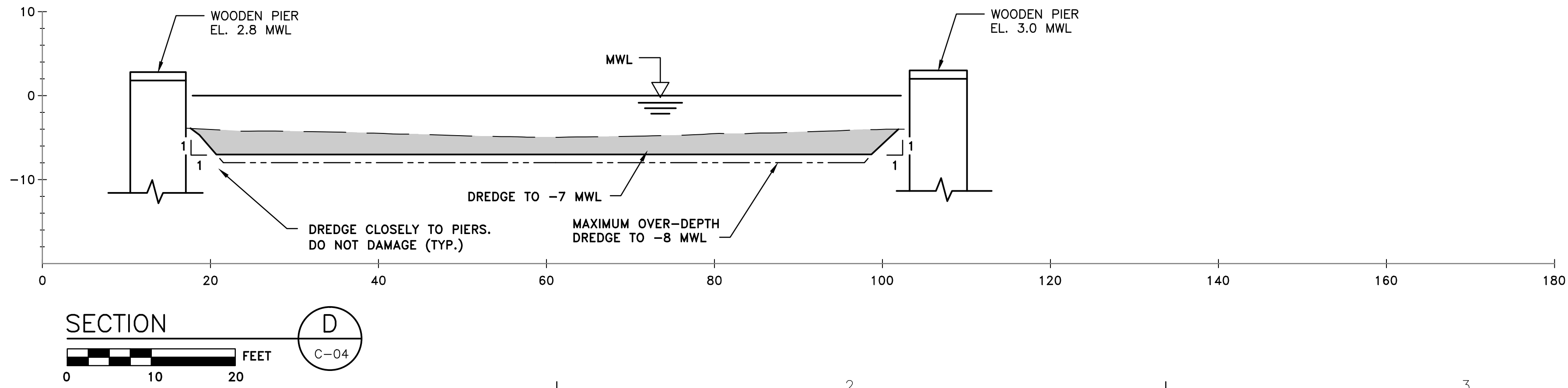
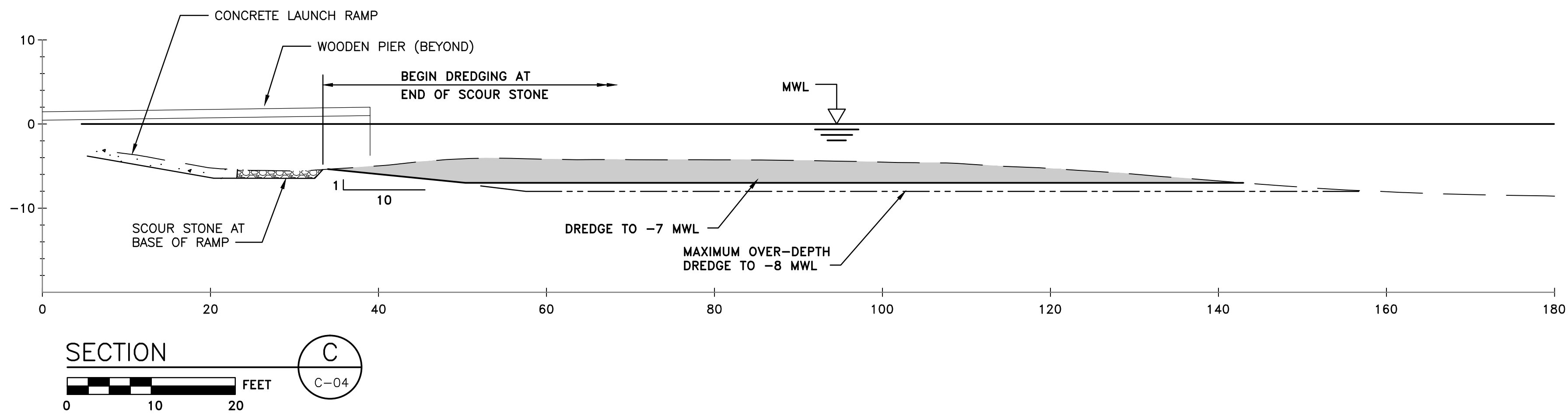
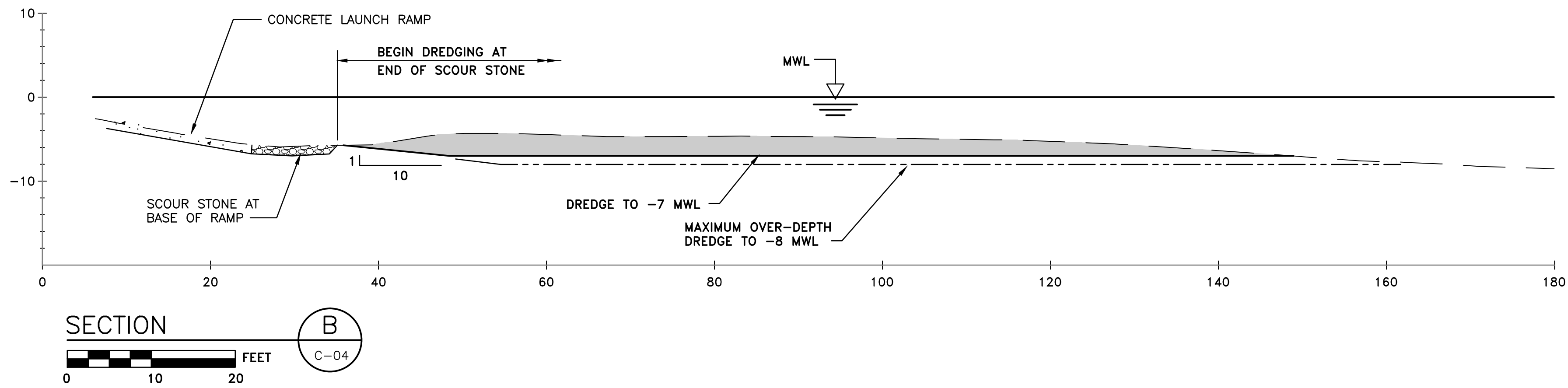
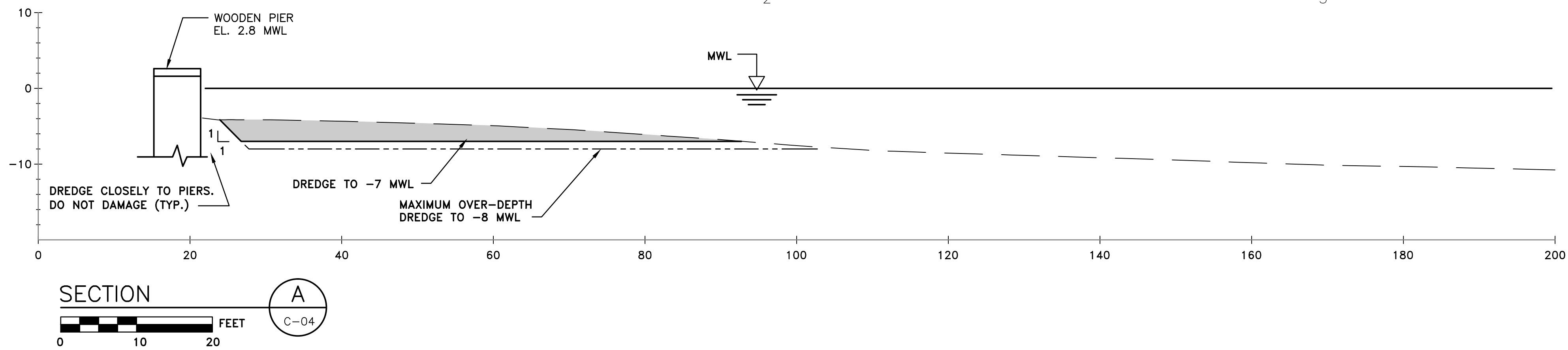
U. S. COAST GUARD
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USCG. CEU CLEVELAND
1240 EAST 9TH STREET
CLEVELAND, OH 44199-2060

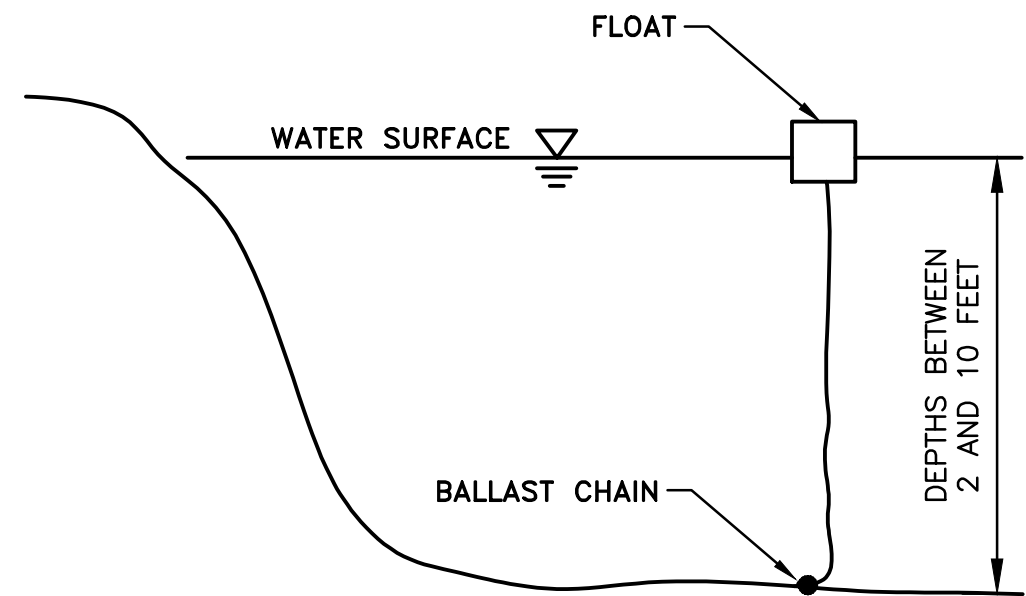
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EDITED BY: MJC		
CHECKED BY: TAP		
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DREDGING AT MWR BOAT RAMP CG BASE ELIZABETH CITY ELIZABETH CITY NC MWR BOAT RAMP CIVIL DREDGE PLAN		
REVIEWED BY: J.W. NICKS	REVIEWED BY: G.P. LODGE	REVIEWED BY: G.S. PLACZEK
PROJECT ENG.	BRANCH CHIEF	TECH. DIRECTOR
P.R. CARROLL, CDR. APPROVING OFFICER		DATE
PROJECT NUMBER 3756419		DRAWING NUMBER 8257-D
DISCIPLINE/SHT NO C-03		SHEET 3 OF 4



LEGEND

- EXISTING TOP OF SEDIMENT
- PROPOSED BASE DREDGE ELEVATION
- MAXIMUM OVER-DEPTH DREDGE

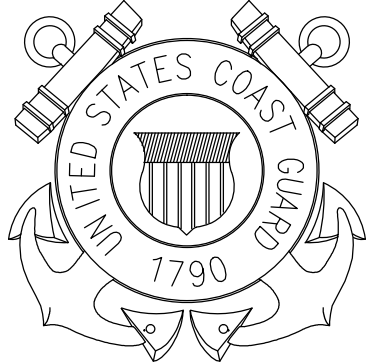


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U. S. COAST GUARD
CIVIL ENGINEERING UNIT
CLEVELAND



USCG. CEU CLEVELAND
1240 EAST 9TH STREET
CLEVELAND, OH 44199-2060

ISSUE		
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DRAWN BY: MJC
EDITED BY: MJC
CHECKED BY: TAP

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SHEET TITLE

DREDGING AT MWR BOAT RAMP
CG BASE ELIZABETH CITY
ELIZABETH CITY NC
MWR BOAT RAMP
CIVIL
DREDGE CROSS SECTIONS

REVIEWED BY:	REVIEWED BY:	REVIEWED BY:
J.W. NICKS	G.P. LODGE	G.S. PLACZEK
PROJECT ENG.	BRANCH CHIEF	TECH. DIRECTOR

P.R. CARROLL, CDR.	DATE
APPROVING OFFICER	

PROJECT NUMBER	DRAWING NUMBER
3756419	8257-D
DISCIPLINE/SHT NO	SHEET 4 OF 4

C-04

ATTACHMENT (E)

TABLE 1 – LISTING OF RARE, THREATENED AND ENDANGERED SPECIES

**Table 1 - Rare, Threatened and Endangered Species, Natural Communities, and Special Habitats
Listed by the Natural Heritage Program in Elizabeth City Quadrangle and
US Fish and Wildlife Service in Pasquotank County**

Taxonomic Group	Scientific Name	Common Name	State Status	NCNHP Search by USGS Quadrangle	Federal Status	Federal Record Status	Listed in Pasquotank County?	Habitat
Species								
Bird	<i>Haliaeetus leucocephalus</i>	Bald Eagle	T	Historical	BGPA	*Current	yes	mature forests near large bodies of water (for nesting); lakes and sounds (for foraging) [nesting sites only]
Bird	<i>Calidris canutus rufa</i>	Red Knot	-	-	P	*Current	yes	migration stops along the Atlantic coast; principally found in marine and estuarine habitats; main food source is horseshoe crab eggs
Fish	<i>Acipenser brevirostrum</i>	Shortnose Sturgeon	E	Current	E	*Current	yes	anadromous; prefer nearshore marine, estuarine, and riverine habitat of large river systems; migrate periodically into faster moving fresh water areas to spawn
Fish	<i>Acipenser oxyrinchus</i>	Atlantic Sturgeon	SC	Current	E	*Current	yes	anadromous; migrate from estuarine and marine waters into freshwater in the spring and early summer to spawn; spawn in moderately flowing water in deep parts of large rivers; subadults and adults live in coastal waters and estuaries
Fish	<i>Anguilla rostrata</i>	American Eel	-	-	FSC	*Current	yes	catadromous; mature eels migrate from brackish and freshwater habitats in streams, large rivers, and estuaries to the ocean to spawn. Larvae develop in the ocean and the eel migrates upstream to estuaries and freshwater habitats as it matures.
Mammal	<i>Trichechus manatus</i>	West Indian Manatee	-	-	E	*Current	yes	warm waters of estuaries and river mouths
Vascular Plant	<i>Sagittaria weatherbiana</i>	Grassleaf Arrowhead	-	-	FSC	**Historic	yes	fresh or brackish marshes, streambanks, and wet depressions
Vascular Plant	<i>Boltonia asteroides var. glastifolia</i>	White Doll's-daisy	SR-O	Historical	-	-	-	clay-based Carolina bays, marshes, savannas
Vascular Plant	<i>Iris prismatica</i>	Slender Blue Iris	SR-T	Historical	-	-	-	bogs, marshes, and wet powerline clearings
Vascular Plant	<i>Ludwigia alata</i>	Winged Seedbox	SR-P	Historical	-	-	-	interdune ponds, marshes
Habitats								
Natural Community	-	Mesic Mixed Hardwood Forest (Coastal Plain Subtype)	N/A	Current	N/A	-	-	-
Natural Community	-	Nonriverine Wet Hardwood Forest (Oak Flat Subtype)	N/A	Current	N/A	-	-	-
Natural Community	-	Peatland Atlantic White Cedar Forest	N/A	Current	N/A	-	-	-
Natural Community	-	Tidal Swamp (Cypress-Gum Subtype)	N/A	Current	N/A	-	-	-
Notes				State Status:			Federal Status:	
1. Current record - there is at least one record for the element in the region that has been seen recently.				E = Endangered			E = Endangered	
				T = Threatened			T = Threatened	
2. Historical record - either the element has not been found in recent surveys in the region; or it has not been surveyed recently enough to be confident they are still present; or the occurrence is thought to be destroyed.				SC = Special Concern			FSC = Federal Species of Concern	
3. *Current record - the species has been observed in the county within the last 50 years.				SR-O = Significantly Rare - Other			BGPA = Bald and Golden Eagle Protection Act	
4. **Historic record - the species was last observed in the county more than 50 years ago.				SR-P = Significantly Rare - Peripheral			P = Proposed	
				SR-T = Significantly Rare - Throughout			- = Not listed by USFWS in Pasquotank County	
				- = Not listed by NCNHP in Elizabeth City quad			N/A = Not applicable	
				N/A = Not applicable				

ATTACHMENT (F)

REFERENCES CITED

References Cited

North Carolina Natural Heritage Program, Heritage Data Search, 2014. Accessed online September 2014 at <http://www.ncnhp.org/web/nhp/database-search>

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U.S. Fish and Wildlife Service, Endangered Species, Threatened Species, Federal Species of Concern, and Candidate Species for Pasquotank County, 2014. Accessed online September 2014 at <http://www.fws.gov/raleigh/species/cntylist/pasquotank.html>

U.S. Fish and Wildlife Service, Information, Planning, and Conservation System, 2014. Accessed online October 2014 at <http://ecos.fws.gov/ipac/>



North Carolina Department of Environment and Natural Resources
Office of Land and Water Stewardship

Pat McCrory
Governor

Bryan Gossage
Director

John E. Skvarla, III
Secretary

November 13, 2014

Ms. Jennifer Jones
AECOM, Inc.
701 Corporate Center Drive, Suite 475
Raleigh, North Carolina 27607
Jennifer.Jones@aecom.com

RE: US Coast Guard Base – Dredging at the Morale, Welfare, and Recreate (MWR) Boat Ramp
Elizabeth City, Pasquotank County, North Carolina

Dear Ms. Jones:

Thank you for the opportunity to provide information from the North Carolina Natural Heritage Program (NCNHP) database for the proposed project referenced above. The NCNHP database does not show any records for rare species, important natural communities, natural areas, or conservation/managed areas within the proposed project area. Within one mile of the proposed project area, the NCNHP database shows element occurrence records for the following rare species:

SCIENTIFIC NAME	COMMON NAME	ELEMENT OCCURRENCE STATUS*	ACCURACY	STATE STATUS*	FEDERAL STATUS*
<i>Acipenser brevirostrum</i>	Shortnose Sturgeon	Current	Very Low	E	E
<i>Acipenser oxyrinchus</i>	Atlantic Sturgeon	Current	Low	SC	E
<i>Iris prismatica</i>	Slender Blue Iris	Historical	Very Low	SR-T	---

* For status definitions, please see the Help document at <http://www.ncnhp.org/web/nhp/database-search>.

The occurrence records for Shortnose Sturgeon and Atlantic Sturgeon are located in Albemarle Sound and North Carolina coastal waters (including Albemarle Sound), respectively. The occurrence record for Slender Blue Iris is from Hogs Creek in Pasquotank County.

Please note that although the NCNHP database may not show records for rare species within the proposed project area, it does not necessarily mean that they are not present; it may simply mean that the area has not been surveyed. The use of Natural Heritage Program data should not be substituted for actual field surveys if needed, particularly if the project area contains suitable habitat for rare species.

Feel free to contact me at 919-707-8629 or Allison.Weakley@ncdenr.gov if you have questions or need additional information.

Sincerely,

Allison Schwarz Weakley, Conservation Planner
NC Natural Heritage Program



AECOM
701 Corporate Center Drive, Suite 475
Raleigh, NC 27607

919.854.6200 tel
919.854.6259 fax

October 29, 2014

Mr. Pete Benjamin
Field Supervisor
U.S. Fish and Wildlife Service
551F Pylon Drive
Raleigh, NC 27606

Subject: U.S. Coast Guard Base Elizabeth City – Dredging at MWR Boat Ramp, Elizabeth City, North Carolina

Dear Mr. Benjamin:

The U.S. Coast Guard (USCG) Civil Engineering Unit Cleveland is in the process of preparing an Environmental Assessment (EA) for proposed maintenance dredging of the boat basin and moorings at the Morale, Welfare and Recreate (MWR) Boat Ramp at the USCG Base Elizabeth City. The USCG Base Elizabeth City is located approximately 3.5 miles southeast of Elizabeth City, along Highway 34, on the south shore of the Pasquotank River in Pasquotank County, North Carolina (**Attachment A**). The geographic location of the MWR Boat Ramp is latitude 36° 16' 14.72" north and longitude 76° 10' 40.31" west.

The MWR Boat Ramp is a previously dug man-made basin and the proposed dredging will occur entirely within the previously disturbed footprint of the boat basin and moorings. However, the USCG has no record of the as-built drawings indicating the original dredge depth. While the dredging required for the original construction of the MWR Boat Ramp likely disturbed the sediments to a depth proximate to the proposed dredge depth, the exact depth of the original dredging is not documented. Therefore, the USCG has elected to conduct an EA to address National Environmental Policy Act of 1969 requirements. The EA will determine whether or not there is potential for significant impact to the human and natural environments. Following completion of the EA, the USCG will either file a Finding of No Significant Impact or prepare an Environmental Impact Statement. This letter provides the U.S. Fish and Wildlife Service (USFWS) with information on the boat ramp construction, existing environment, and proposed action in advance of issuance of the EA for this project. The USCG would appreciate any comments the USFWS may have for consideration in preparation of the EA.

The MWR Boat Ramp is located on the north end of the Base's shoreline, in a portion of the river tidal flats. The MWR Boat Ramp consists of a two-lane concrete ramp with a concrete pavement approach and a launch dock centered in the boat ramp (**Attachments B and C**). Rubble mound stone breakwaters project approximately 100 to 125 feet from the shoreline on both sides of the launch ramp to form a protected harbor area for launching and retrieving boats. Timber docks line both sides of the boat basin. Mooring piles are in place along the west timber dock. Navigation aids are located near the entrance to the boat basin. These consist of a red dayboard with a solar-powered red beacon, and a green dayboard. The MWR Boat Ramp is utilized by base military personnel for launching privately-owned recreational boats, and for launching USCG vessels stationed at the Building 43 moorings.

The purpose of this project is to dredge the boat basin and moorings to a required depth below mean water level at the MWR Boat Ramp due to changing conditions at the site. Water depth in the boat basin and moorings is reduced by heavy winds from the north, which push water from the boat basin and down the Pasquotank River. This wind effect decreases the depth of the water and

encumbers the USCG's ability to utilize the existing boat ramp for both operational missions and recreational purposes. The project involves mechanical dredging of the boat basin and moorings to a maximum depth of 8 feet below mean water level (includes 1 foot over dredge depth). Mean water level corresponds to elevation -0.44 North American Vertical Datum 1988 (NAVD88). Therefore, dredging will proceed to -8.44 feet NAVD88. The calculated dredge volume for the MWR Boat Ramp is 755 cubic yards of sediment. The proposed dredging project will only temporarily impact open waters (an area totaling approximately 0.18 acres) and will occur entirely within the existing boat basin (limited to an area approximately 150 feet from the shoreline). The 35 percent civil site plan entitled, "Dredging at MWR Boat Ramp: CG Base Elizabeth City" illustrates the location and specifications of this proposed dredging project (**Attachment D**).

During the proposed dredging activities, the dredge area will be closed with an impermeable floating turbidity curtain with a weighted bottom edge. The dredge material will be deposited on barges, dewatered on the barges, transported by barge to an unloading site, offloaded to trucks, and transported by truck to the East Carolina Regional Landfill in Aulander, North Carolina for disposal. Formal approval for disposal of the dredge material at the MWR Boat Ramp has been received from the East Carolina Regional Landfill.

The USCG has reviewed the USFWS Threatened and Endangered Species and Species of Concern for Pasquotank County and the North Carolina Natural Heritage Program listed species for the Elizabeth City Quadrangle along with their habitat requirements. Table 1 is a list of rare, threatened and endangered species documented to occur in Pasquotank County and the Elizabeth City Quadrangle where the project site is located (**Attachment E**). The USCG also reviewed the USFWS Critical Habitat Mapper and the online USFWS Information, Planning, and Conservation (IPaC) System.

According to the online resources, there are three federally-listed endangered species identified within Pasquotank County with potential habitat in the project area: Shortnose sturgeon (*Acipenser brevirostrum*), Atlantic sturgeon (*Acipenser oxyrinchus*), and West Indian manatee (*Trichechus manatus*). Additionally, one federally-listed proposed threatened species (Red knot; *Calidris canutus rufa*) and one protected species under the Bald and Golden Eagle Protection Act (Bald eagle; *Haliaeetus leucocephalus*) were identified within Pasquotank County. Review of the IPaC System indicates that only two of the endangered species (i.e., Shortnose sturgeon and Atlantic sturgeon) and the proposed threatened species (i.e., Red knot) should be considered in an effects analysis for the proposed project. No critical habitats were identified within the proposed project area.

The USCG requests that the USFWS provide any additional information or potential concerns regarding the presence of threatened and endangered species that may be potentially affected by the maintenance dredging of the boat basin and moorings at the MWR Boat Ramp. Any comments or recommendations that your agency may have for avoiding potential impacts associated with this project would also be appreciated. We would appreciate receipt of your written approval within 30 days of this letter; this will help us maintain the overall schedule for this project.

If you have any questions, please contact Jennifer Jones (AECOM) at (919) 239-7150 or Leilani Woods (USCG) at (252) 335-6847.

Yours sincerely,



Jennifer Jones
Environmental Task Manager



Ron Johnson
Senior Biologist

Cc: Ms. Leilani Woods, Environmental Engineer, USCG Civil Engineering Unit Cleveland
Mr. Greg Carpenter, Environmental Project Manager, USCG Civil Engineering Unit Cleveland

Attachments: (A) Dredge Site – Location Map
(B) Dredge Site – Aerial Photograph
(C) Dredge Site – Ground-Level Photographs
(D) Dredge Site – 35% Plan
(E) Table 1 – Listing of Rare, Threatened and Endangered Species
(F) References Cited

ATTACHMENT (A)

DREDGE SITE - LOCATION MAP



Map Location



Site Location Map

MWR Boat Ramp
USCG Base Elizabeth City, North Carolina
Elizabeth City NC Topographic Quadrangle (1983)

0 1,000 2,000
Feet

June 2014

60321114

Figure 1

AECOM

AECOM Environment
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Raleigh, NC 27607
Phone: (919) 854-6200
Fax: (919) 854-6259
Web: <http://www.aecom.com>

ATTACHMENT (B)

DREDGE SITE – AERIAL PHOTOGRAPH



Scale: Not To Scale

(Source: Google Earth© 2014)

Dredge Site - Aerial Photograph

US Coast Guard
Base Elizabeth City – MWR Boat Ramp
Elizabeth City, NC

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Attachment B

www.aecom.com

ATTACHMENT (C)

DREDGE SITE – GROUND-LEVEL PHOTOGRAPHS

Facility Name: United States Coast Guard	Site Location: MWR Boat Ramp – USCG Base Elizabeth City, NC	Project No. 60321114
--	---	--------------------------------

Photo No. 1	Date: 05/7/14	
Direction Photo Taken: North		
Description: View from the southern side of the shore (concrete pavement approach) of the concrete ramp, docks, navigation aids, and light pedestals.		

Photo No. 2	Date: 05/7/14	
Direction Photo Taken: Northwest		
Description: View from the concrete pavement approach along the southern shoreline.		

Facility Name: United States Coast Guard	Site Location: MWR Boat Ramp – USCG Base Elizabeth City, NC	Project No. 60321114
--	---	--------------------------------

Photo No. 3	Date: 05/7/14	
Direction Photo Taken: South		
Description: View looking south of the concrete ramp, docks, light pedestals, and mooring piles.		

Photo No. 4	Date: 05/7/14
Direction Photo Taken: North	
Description: View at the end of the concrete ramp of the docks, light pedestals, and mooring piles.	



Facility Name: United States Coast Guard	Site Location: MWR Boat Ramp – USCG Base Elizabeth City, NC	Project No. 60321114
--	---	--------------------------------

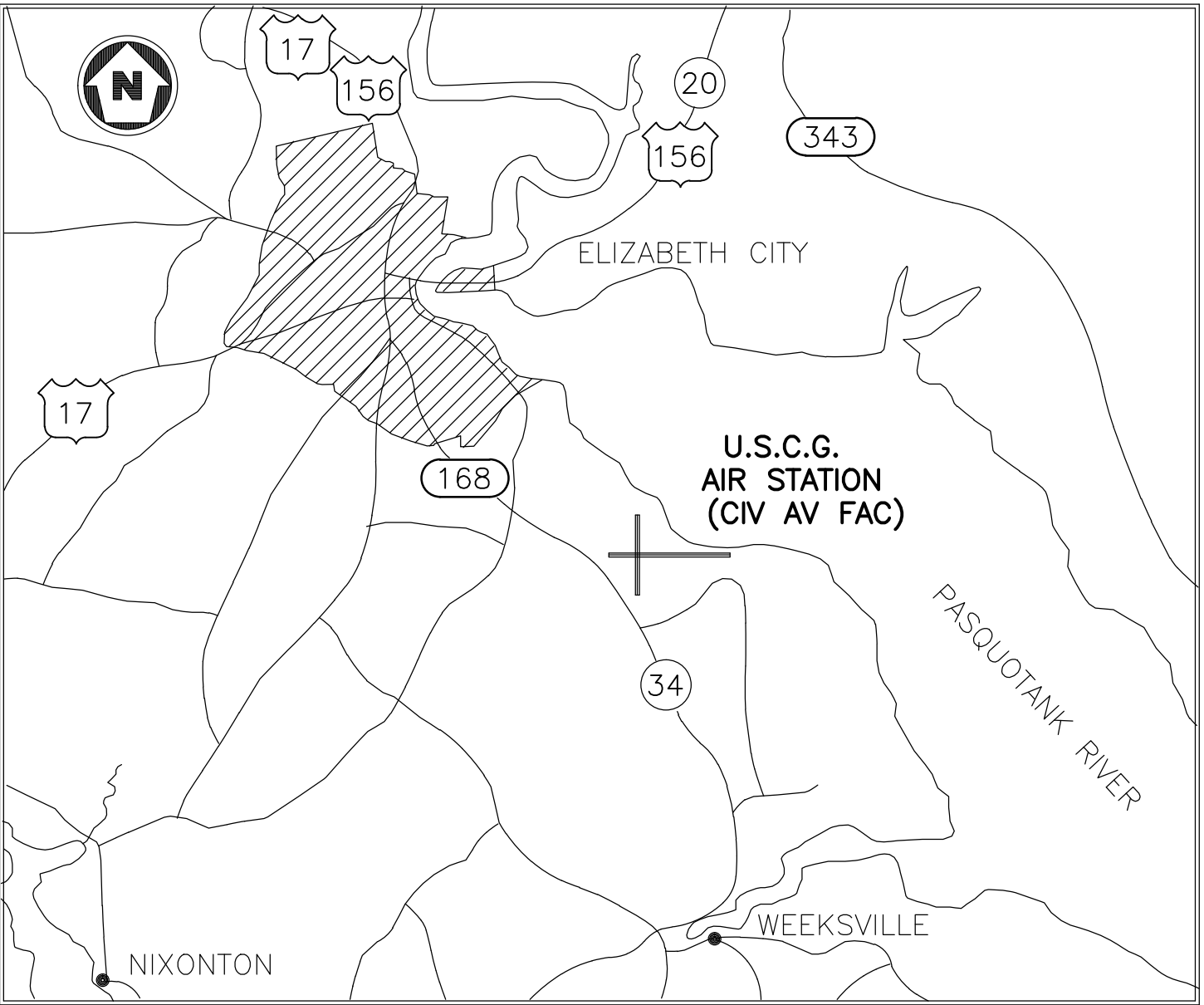
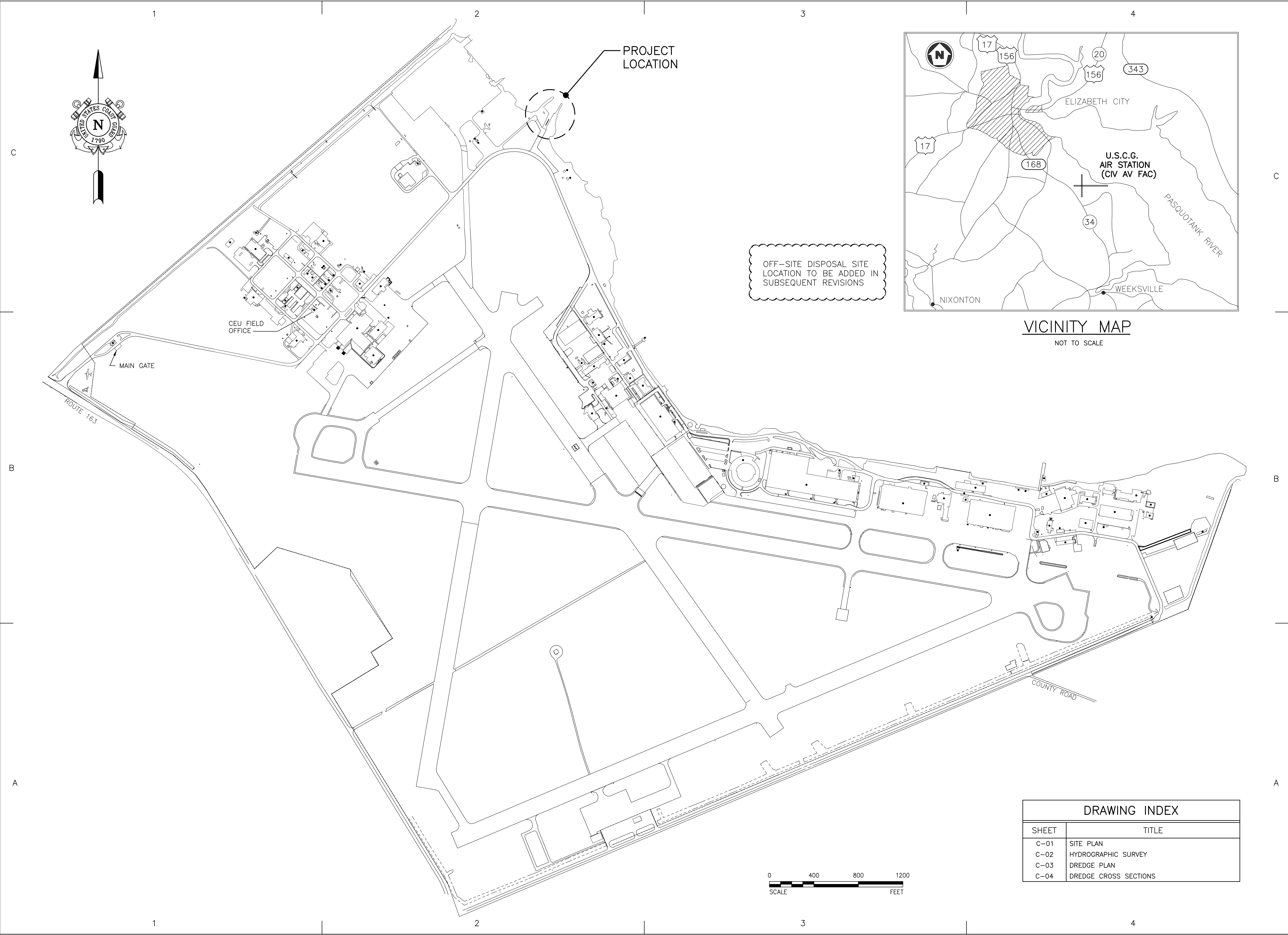
Photo No. 5	Date: 05/7/14
Direction Photo Taken: South-Southeast	
Description: View of the man-made beach located southeast of the MWR Boat Ramp.	



Photo No. 6	Date: 05/7/14	
Direction Photo Taken: North		
Description: View of the navigation aids at entry to the protected harbor area. Note two timbers low to water surface in the entry-way.		

ATTACHMENT (D)

DREDGE SITE – 35% PLAN



VICINITY MAP

NOT TO SCALE

DRAWING INDEX

SHEET	TITLE
C-01	SITE PLAN
C-02	HYDROGRAPHIC SURVEY
C-03	DREDGE PLAN
C-04	DREDGE CROSS SECTIONS

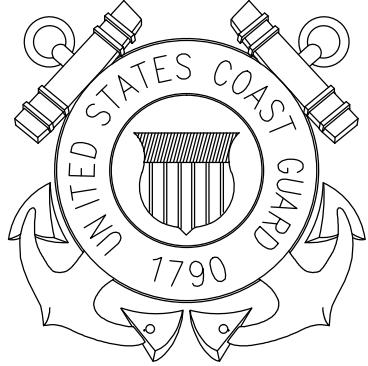
USCG CEU
CLEVELAND, OHIO
(216) 902-6200

CONSULTANTS

AECOM

8320 Guilford Road, Suite L
Columbia, MD 21046

U. S. COAST GUARD
CIVIL ENGINEERING UNIT
CLEVELAND



USCG. CEU CLEVELAND
1240 EAST 9TH STREET
CLEVELAND, OH 44199-2060

ISSUE		
MARK	DATE	DESCRIPTION

A/E PROJECT NO: 60321114
CAD FILE NAME: C8257C01.DWG
DESIGNED BY: SJR
DRAWN BY: MJC
EDITED BY: MJC
CHECKED BY: TAP

SCALE: AS SHOWN PLOT SCALE: 1:1

DREDGING AT MWR BOAT RAMP
CG BASE ELIZABETH CITY
ELIZABETH CITY
MWR BOAT RAMP
CIVIL
SITE PLAN

NC

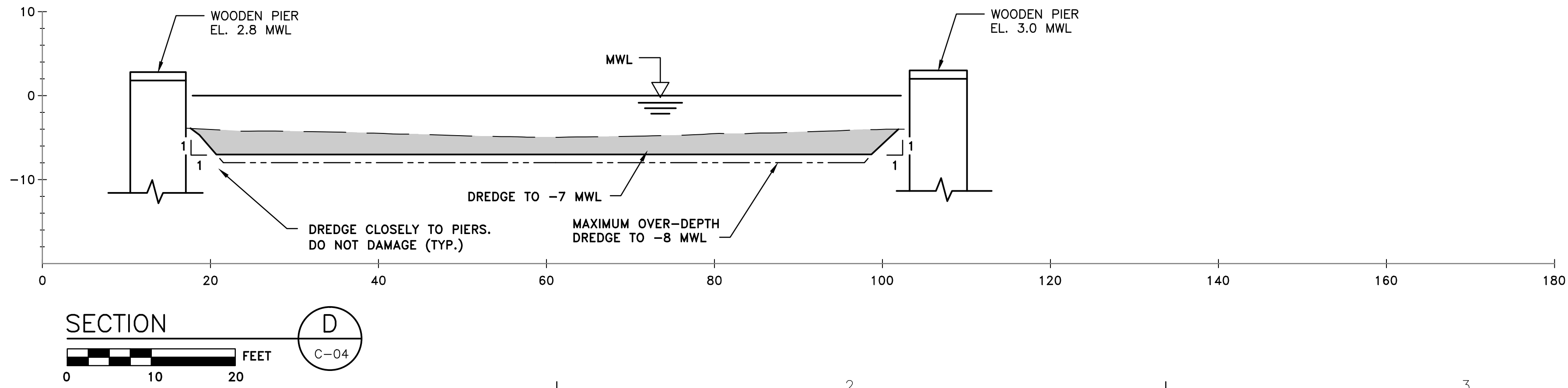
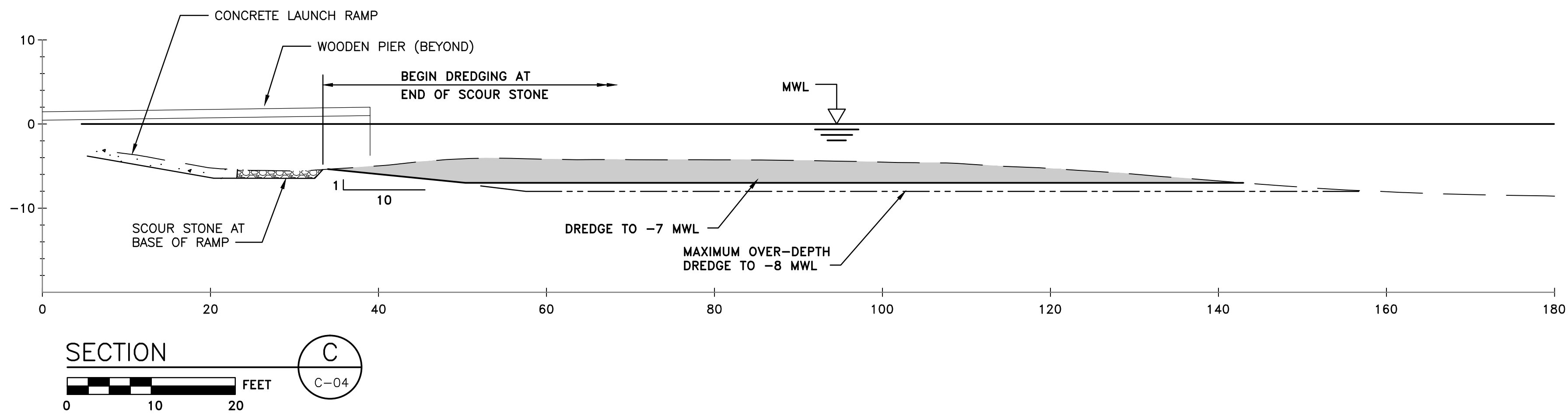
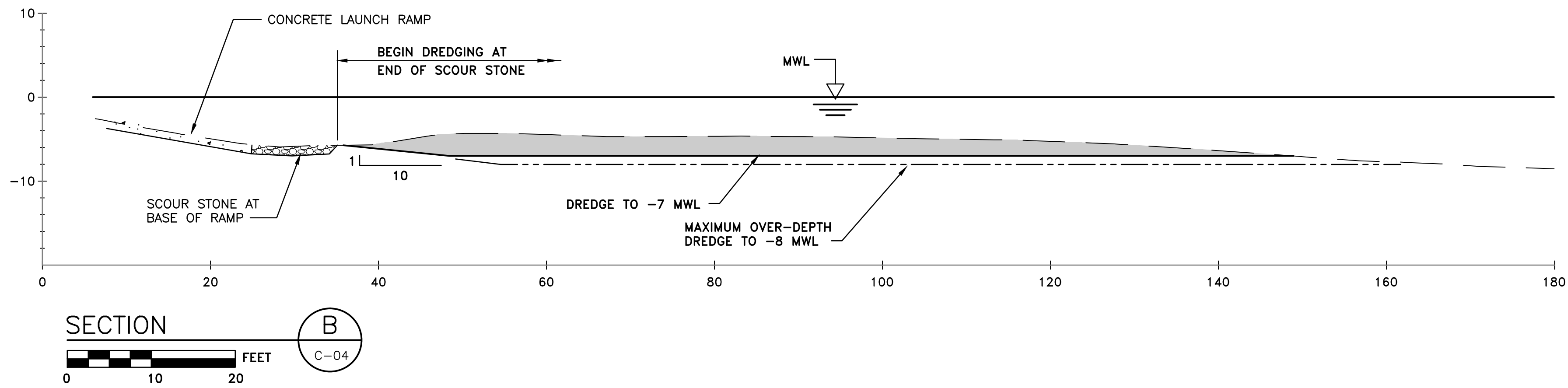
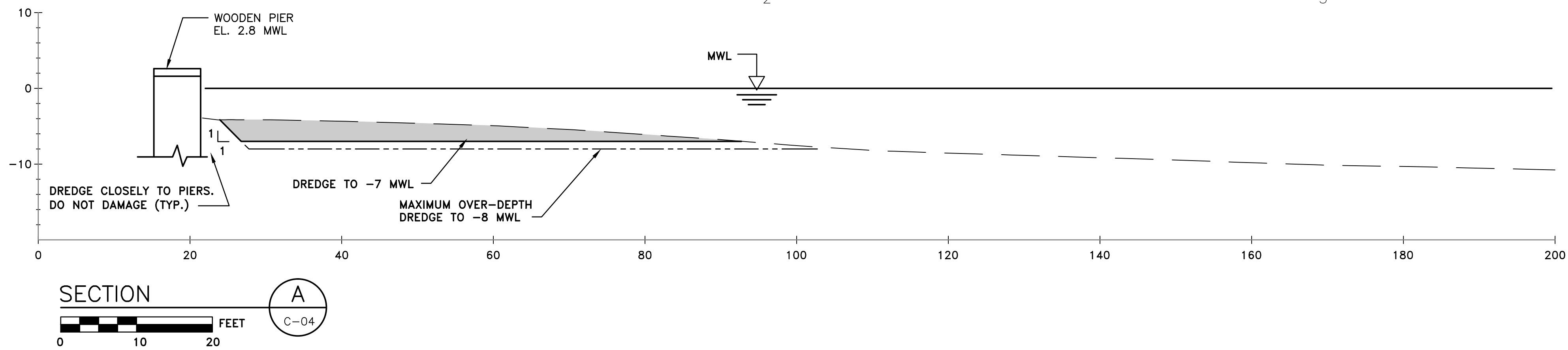
REVIEWED BY: J.W. NICKS PROJECT ENG.	REVIEWED BY: G.P. LODGE BRANCH CHIEF	REVIEWED BY: G.S. PLACZEK TECH. DIRECTOR
--	--	--

P.R. CARROLL, CDR.
APPROVING OFFICER

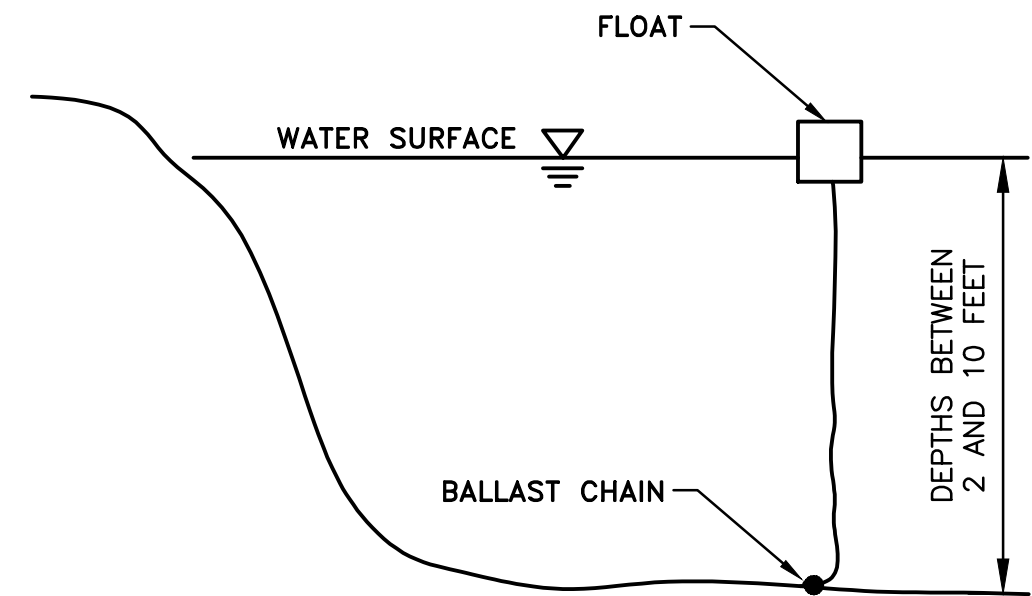
DATE

PROJECT NUMBER	DRAWING NUMBER
3756419	8257-D
DISCIPLINE/SHT NO	SHEET 1 OF 4
C-01	

A horizontal scale bar with tick marks at 0, 16, 32, and 48 feet. The bar is divided into segments: a white segment from 0 to 16, a black segment from 16 to 32, and a white segment from 32 to 48. Below the bar is the text "SCALE IN FEET".



LEGEND	
---	EXISTING TOP OF SEDIMENT
---	PROPOSED BASE DREDGE ELEVATION
---	MAXIMUM OVER-DEPTH DREDGE



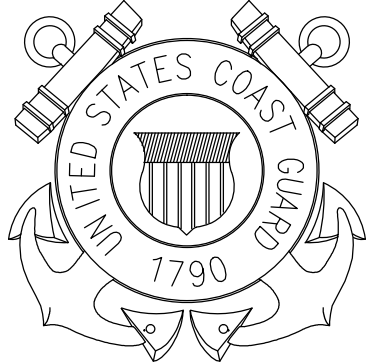
USCG CEU
CLEVELAND, OHIO
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U. S. COAST GUARD
CIVIL ENGINEERING UNIT
CLEVELAND



USCG. CEU CLEVELAND
1240 EAST 9TH STREET
CLEVELAND, OH 44199-2060

ISSUE		
MARK	DATE	DESCRIPTION

A/E PROJECT NO: 60321114
CAD FILE NAME: C8257C04.DWG
DESIGNED BY: SJR
DRAWN BY: MJC
EDITED BY: MJC
CHECKED BY: TAP

SCALE: AS SHOWN PLOT SCALE: 1:1

SHEET TITLE

DREDGING AT MWR BOAT RAMP
CG BASE ELIZABETH CITY
ELIZABETH CITY NC
MWR BOAT RAMP
CIVIL
DREDGE CROSS SECTIONS

REVIEWED BY:	REVIEWED BY:	REVIEWED BY:
J.W. NICKS	G.P. LODGE	G.S. PLACZEK
PROJECT ENG.	BRANCH CHIEF	TECH. DIRECTOR

P.R. CARROLL, CDR.
APPROVING OFFICER

DATE

PROJECT NUMBER	DRAWING NUMBER
3756419	8257-D
DISCIPLINE/SHT NO	SHEET 4 OF 4
C-04	

ATTACHMENT (E)

TABLE 1 – LISTING OF RARE, THREATENED AND ENDANGERED SPECIES

**Table 1 - Rare, Threatened and Endangered Species, Natural Communities, and Special Habitats
Listed by the Natural Heritage Program in Elizabeth City Quadrangle and
US Fish and Wildlife Service in Pasquotank County**

Taxonomic Group	Scientific Name	Common Name	State Status	NCNHP Search by USGS Quadrangle	Federal Status	Federal Record Status	Listed in Pasquotank County?	Habitat
Species								
Bird	<i>Haliaeetus leucocephalus</i>	Bald Eagle	T	Historical	BGPA	*Current	yes	mature forests near large bodies of water (for nesting); lakes and sounds (for foraging) [nesting sites only]
Bird	<i>Calidris canutus rufa</i>	Red Knot	-	-	P	*Current	yes	migration stops along the Atlantic coast; principally found in marine and estuarine habitats; main food source is horseshoe crab eggs
Fish	<i>Acipenser brevirostrum</i>	Shortnose Sturgeon	E	Current	E	*Current	yes	anadromous; prefer nearshore marine, estuarine, and riverine habitat of large river systems; migrate periodically into faster moving fresh water areas to spawn
Fish	<i>Acipenser oxyrinchus</i>	Atlantic Sturgeon	SC	Current	E	*Current	yes	anadromous; migrate from estuarine and marine waters into freshwater in the spring and early summer to spawn; spawn in moderately flowing water in deep parts of large rivers; subadults and adults live in coastal waters and estuaries
Fish	<i>Anguilla rostrata</i>	American Eel	-	-	FSC	*Current	yes	catadromous; mature eels migrate from brackish and freshwater habitats in streams, large rivers, and estuaries to the ocean to spawn. Larvae develop in the ocean and the eel migrates upstream to estuaries and freshwater habitats as it matures.
Mammal	<i>Trichechus manatus</i>	West Indian Manatee	-	-	E	*Current	yes	warm waters of estuaries and river mouths
Vascular Plant	<i>Sagittaria weatherbiana</i>	Grassleaf Arrowhead	-	-	FSC	**Historic	yes	fresh or brackish marshes, streambanks, and wet depressions
Vascular Plant	<i>Boltonia asteroides var. glastifolia</i>	White Doll's-daisy	SR-O	Historical	-	-	-	clay-based Carolina bays, marshes, savannas
Vascular Plant	<i>Iris prismatica</i>	Slender Blue Iris	SR-T	Historical	-	-	-	bogs, marshes, and wet powerline clearings
Vascular Plant	<i>Ludwigia alata</i>	Winged Seedbox	SR-P	Historical	-	-	-	interdune ponds, marshes
Habitats								
Natural Community	-	Mesic Mixed Hardwood Forest (Coastal Plain Subtype)	N/A	Current	N/A	-	-	-
Natural Community	-	Nonriverine Wet Hardwood Forest (Oak Flat Subtype)	N/A	Current	N/A	-	-	-
Natural Community	-	Peatland Atlantic White Cedar Forest	N/A	Current	N/A	-	-	-
Natural Community	-	Tidal Swamp (Cypress-Gum Subtype)	N/A	Current	N/A	-	-	-
Notes				State Status:			Federal Status:	
1. Current record - there is at least one record for the element in the region that has been seen recently.				E = Endangered			E = Endangered	
				T = Threatened			T = Threatened	
2. Historical record - either the element has not been found in recent surveys in the region; or it has not been surveyed recently enough to be confident they are still present; or the occurrence is thought to be destroyed.				SC = Special Concern			FSC = Federal Species of Concern	
3. *Current record - the species has been observed in the county within the last 50 years.				SR-O = Significantly Rare - Other			BGPA = Bald and Golden Eagle Protection Act	
4. **Historic record - the species was last observed in the county more than 50 years ago.				SR-P = Significantly Rare - Peripheral			P = Proposed	
				SR-T = Significantly Rare - Throughout			- = Not listed by USFWS in Pasquotank County	
				- = Not listed by NCNHP in Elizabeth City quad			N/A = Not applicable	
				N/A = Not applicable				

ATTACHMENT (F)

REFERENCES CITED

References Cited

North Carolina Natural Heritage Program, Heritage Data Search, 2014. Accessed online September 2014 at <http://www.ncnhp.org/web/nhp/database-search>

North Carolina OneMap, Geospatial Portal, 2014. Accessed online October 2014 at <http://data.nconemap.com/geoportal/catalog/main/home.page>

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U.S. Fish and Wildlife Service, Information, Planning, and Conservation System, 2014. Accessed online October 2014 at <http://ecos.fws.gov/ipac/>



United States Department of the Interior

FISH AND WILDLIFE SERVICE
Raleigh ES Field Office
Post Office Box 33726
Raleigh, North Carolina 27636-3726

November 20, 2014

Jennifer Jones
AECOM
701 Corporate Center Drive, Suite 475
Raleigh, NC 27607

Re: USCG Base Elizabeth City Dredging at MWR Boat Ramp- Pasquotank County, NC

Dear Ms. Jones:

This letter is to inform you that a list of all federally-protected endangered and threatened species with known occurrences in North Carolina is now available on the U.S. Fish and Wildlife Service's (Service) web page at <http://www.fws.gov/raleigh>. Therefore, if you have projects that occur within the Raleigh Field Office's area of responsibility (see attached county list), you no longer need to contact the Raleigh Field Office for a list of federally-protected species.

Our web page contains a complete and frequently updated list of all endangered and threatened species protected by the provisions of the Endangered Species Act of 1973, as amended (16 U.S.C. 1531 et seq.)(Act), and a list of federal species of concern¹ that are known to occur in each county in North Carolina.

Section 7 of the Act requires that all federal agencies (or their designated non-federal representative), in consultation with the Service, insure that any action federally authorized, funded, or carried out by such agencies is not likely to jeopardize the continued existence of any federally-listed endangered or threatened species. A biological assessment or evaluation may be prepared to fulfill that requirement and in determining whether additional consultation with the Service is necessary. In addition to the federally-protected species list, information on the species' life histories and habitats and information on completing a biological assessment or evaluation can be found on our web page at <http://www.fws.gov/raleigh>. Please check the web site often for updated information or changes.

¹ The term "federal species of concern" refers to those species which the Service believes might be in need of concentrated conservation actions. Federal species of concern receive no legal protection and their designation does not necessarily imply that the species will eventually be proposed for listing as a federally endangered or threatened species. However, we recommend that all practicable measures be taken to avoid or minimize adverse impacts to federal species of concern.

If your project contains suitable habitat for any of the federally-listed species known to be present within the county where your project occurs, the proposed action has the potential to adversely affect those species. As such, we recommend that surveys be conducted to determine the species' presence or absence within the project area. The use of North Carolina Natural Heritage program data should not be substituted for actual field surveys.

If you determine that the proposed action may affect (i.e., likely to adversely affect or not likely to adversely affect) a federally-protected species, you should notify this office with your determination, the results of your surveys, survey methodologies, and an analysis of the effects of the action on listed species, including consideration of direct, indirect, and cumulative effects, before conducting any activities that might affect the species. If you determine that the proposed action will have no effect (i.e., no beneficial or adverse, direct or indirect effect) on federally listed species, then you are not required to contact our office for concurrence (unless an Environmental Impact Statement is prepared). However, you should maintain a complete record of the assessment, including steps leading to your determination of effect, the qualified personnel conducting the assessment, habitat conditions, site photographs, and any other related articles.

With regard to the above-referenced project, we offer the following remarks. Our comments are submitted pursuant to, and in accordance with, provisions of the Endangered Species Act.

Based on the information provided and other information available, it appears that the proposed action is not likely to adversely affect any federally-listed endangered or threatened species, their formally designated critical habitat, or species currently proposed for listing under the Act at these sites. We believe that the requirements of section 7(a)(2) of the Act have been satisfied for your project. Please remember that obligations under section 7 consultation must be reconsidered if: (1) new information reveals impacts of this identified action that may affect listed species or critical habitat in a manner not previously considered; (2) this action is subsequently modified in a manner that was not considered in this review; or, (3) a new species is listed or critical habitat determined that may be affected by the identified action.

However, the Service is concerned about the potential impacts the proposed action might have on aquatic species. Aquatic resources are highly susceptible to sedimentation. Therefore, we recommend that all practicable measures be taken to avoid adverse impacts to aquatic species, including implementing directional boring methods and stringent sediment and erosion control measures. An erosion and sedimentation control plan should be submitted to and approved by the North Carolina Division of Land Resources, Land Quality Section prior to construction. Erosion and sedimentation controls should be installed and maintained between the construction site and any nearby down-gradient surface waters. In addition, we recommend maintaining natural, vegetated buffers on all streams and creeks adjacent to the project site.

The North Carolina Wildlife Resources Commission has developed a Guidance Memorandum (a copy can be found on our website at (<http://www.fws.gov/raleigh>) to address and mitigate secondary and cumulative impacts to aquatic and terrestrial wildlife resources and water quality. We recommend that you consider this document in the development of your projects and in completing an initiation package for consultation (if necessary).

We hope you find our web page useful and informative and that following the process described above will reduce the time required, and eliminate the need, for general correspondence for . species' lists. If you have any questions or comments, please contact John Ellis of this office at (919) 856-4520 ext. 26.

Sincerely,

A handwritten signature in blue ink, appearing to read "Pete Benjamin".

Pete Benjamin
Field Supervisor

List of Counties in the Service's Raleigh Field Office Area of Responsibility

Alamance	Perquimans
Beaufort	Person
Bertie	Pitt
Bladen	Randolph
Brunswick	Richmond
Camden	Robeson
Carteret	Rockingham
Caswell	Sampson
Chatham	Scotland
Chowan	Tyrrell
Columbus	Vance
Craven	Wake
Cumberland	Warren
Currituck	Washington
Dare	Wayne
Duplin	Wilson
Durham	
Edgecombe	
Franklin	
Gates	
Granville	
Greene	
Guilford	
Halifax	
Harnett	
Hertford	
Hoke	
Hyde	
Johnston	
Jones	
Lee	
Lenoir	
Martin	
Montgomery	
Moore	
Nash	
New Hanover	
Northampton	
Onslow	
Orange	
Pamlico	
Pasquotank	
Pender	



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919.854.6259 fax

October 29, 2014

Ms. Shannon Deaton
Habitat Conservation Program Manager
North Carolina Wildlife Resources Commission
Inland Fisheries
1721 Mail Service Center
Raleigh, NC 27699-1721

Subject: U.S. Coast Guard Base Elizabeth City – Dredging at MWR Boat Ramp, Elizabeth City, North Carolina

Dear Ms. Deaton:

The U.S. Coast Guard (USCG) Civil Engineering Unit Cleveland is in the process of preparing an Environmental Assessment (EA) for proposed maintenance dredging of the boat basin and moorings at the Morale, Welfare and Recreate (MWR) Boat Ramp at the USCG Base Elizabeth City. The USCG Base Elizabeth City is located approximately 3.5 miles southeast of Elizabeth City, along Highway 34, on the south shore of the Pasquotank River in Pasquotank County, North Carolina (**Attachment A**). The geographic location of the MWR Boat Ramp is latitude 36° 16' 14.72" north and longitude 76° 10' 40.31" west.

The MWR Boat Ramp is a previously dug man-made basin and the proposed dredging will occur entirely within the previously disturbed footprint of the boat basin and moorings. However, the USCG has no record of the as-built drawings indicating the original dredge depth. While the dredging required for the original construction of the MWR Boat Ramp likely disturbed the sediments to a depth proximate to the proposed dredge depth, the exact depth of the original dredging is not documented. Therefore, the USCG has elected to conduct an EA to address National Environmental Policy Act of 1969 requirements. The EA will determine whether or not there is potential for significant impact to the human and natural environments. Following completion of the EA, the USCG will either file a Finding of No Significant Impact or prepare an Environmental Impact Statement. This letter provides the North Carolina Wildlife Resources Commission (NC WRC) with information on the boat ramp construction, existing environment, and proposed action in advance of issuance of the EA for this project. The USCG would appreciate any comments the NC WRC may have for consideration in preparation of the EA.

The MWR Boat Ramp is located on the north end of the Base's shoreline, in a portion of the river tidal flats. The MWR Boat Ramp consists of a two-lane concrete ramp with a concrete pavement approach and a launch dock centered in the boat ramp (**Attachments B and C**). Rubble mound stone breakwaters project approximately 100 to 125 feet from the shoreline on both sides of the launch ramp to form a protected harbor area for launching and retrieving boats. Timber docks line both sides of the boat basin. Mooring piles are in place along the west timber dock. Navigation aids are located near the entrance to the boat basin. These consist of a red dayboard with a solar-powered red beacon, and a green dayboard. The MWR Boat Ramp is utilized by base military personnel for launching privately-owned recreational boats, and for launching USCG vessels stationed at the Building 43 moorings.

The purpose of this project is to dredge the boat basin and moorings to a required depth below mean water level at the MWR Boat Ramp due to changing conditions at the site. Water depth in the boat basin and moorings is reduced by heavy winds from the north, which push water from the boat

basin and down the Pasquotank River. This wind effect decreases the depth of the water and encumbers the USCG's ability to utilize the existing boat ramp for both operational missions and recreational purposes. The project involves mechanical dredging of the boat basin and moorings to a maximum depth of 8 feet below mean water level (includes 1 foot over dredge depth). Mean water level corresponds to elevation -0.44 North American Vertical Datum 1988 (NAVD88). Therefore, dredging will proceed to -8.44 feet NAVD88. The calculated dredge volume for the MWR Boat Ramp is 755 cubic yards of sediment. The proposed dredging project will only temporarily impact open waters (an area totaling approximately 0.18 acres) and will occur entirely within the existing boat basin (limited to an area approximately 150 feet from the shoreline). The 35 percent civil site plan entitled, "Dredging at MWR Boat Ramp: CG Base Elizabeth City" illustrates the location and specifications of this proposed dredging project (**Attachment D**).

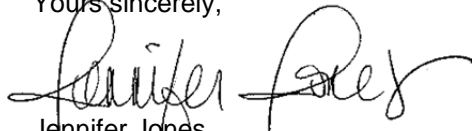
During the proposed dredging activities, the dredge area will be closed with an impermeable floating turbidity curtain with a weighted bottom edge. The dredge material will be deposited on barges, dewatered on the barges, transported by barge to an unloading site, offloaded to trucks, and transported by truck to the East Carolina Regional Landfill in Aulander, North Carolina for disposal. Formal approval for disposal of the dredge material at the MWR Boat Ramp has been received from the East Carolina Regional Landfill.

The USCG has completed background research and records review using various maps and available online sources for the site including, but not limited to: the North Carolina OneMap (NC OneMap) Habitat Map; the NC DMF Anadromous Fish Spawning Areas (AFSA) map for the Albemarle Sound area; the DMF Fishery Nursery Areas map; the National Oceanic and Atmospheric Administration Environmental Sensitivity Map for the Elizabeth City Quadrangle; and the South Atlantic Fishery Management Council's Essential Fish Habitat (EFH) online map. Based on the USCG's review, the proposed project site is located within AFSA waters and in an area of EFH; however, the project site is not located in the vicinity of a designated submerged aquatic vegetation area, a primary nursery area, or an EFH-Habitat Area of Particular Concern.

The USCG requests that the NC WRC provide any additional information or potential concerns regarding the presence of significant natural resources that may be potentially affected by the maintenance dredging of the boat basin and moorings at the MWR Boat Ramp. Any comments or recommendations that your agency may have for avoiding potential impacts associated with this project would also be appreciated. We would appreciate receipt of your written approval within 30 days of this letter; this will help us maintain the overall schedule for this project.

If you have any questions, please contact Jennifer Jones (AECOM) at (919) 239-7150 or Leilani Woods (USCG) at (252) 335-6847.

Yours sincerely,



Jennifer Jones
Environmental Task Manager



Ron Johnson
Senior Biologist

Cc: Ms. Leilani Woods, Environmental Engineer, USCG Civil Engineering Unit Cleveland
Mr. Greg Carpenter, Environmental Project Manager, USCG Civil Engineering Unit Cleveland

Attachments: (A) Dredge Site - Location Map
(B) Dredge Site - Aerial Photograph
(C) Dredge Site - Ground-Level Photographs
(D) Dredge Site – 35% Plan
(E) References Cited

ATTACHMENT (A)

DREDGE SITE - LOCATION MAP



Map Location



Site Location Map

MWR Boat Ramp
USCG Base Elizabeth City, North Carolina
Elizabeth City NC Topographic Quadrangle (1983)

0 1,000 2,000
Feet

June 2014

60321114

Figure 1

AECOM

AECOM Environment
701 Corporate Center Drive, Suite 475
Raleigh, NC 27607
Phone: (919) 854-6200
Fax: (919) 854-6259
Web: <http://www.aecom.com>

ATTACHMENT (B)

DREDGE SITE – AERIAL PHOTOGRAPH



Scale: Not To Scale

(Source: Google Earth© 2014)

Dredge Site - Aerial Photograph

US Coast Guard
Base Elizabeth City – MWR Boat Ramp
Elizabeth City, NC

AECOM

Attachment B

www.aecom.com

ATTACHMENT (C)

DREDGE SITE – GROUND-LEVEL PHOTOGRAPHS

Facility Name: United States Coast Guard	Site Location: MWR Boat Ramp – USCG Base Elizabeth City, NC	Project No. 60321114
--	---	--------------------------------

Photo No. 1	Date: 05/7/14	
Direction Photo Taken: North		
Description: View from the southern side of the shore (concrete pavement approach) of the concrete ramp, docks, navigation aids, and light pedestals.		

Photo No. 2	Date: 05/7/14	
Direction Photo Taken: Northwest		
Description: View from the concrete pavement approach along the southern shoreline.		

Facility Name: United States Coast Guard	Site Location: MWR Boat Ramp – USCG Base Elizabeth City, NC	Project No. 60321114
--	---	--------------------------------

Photo No. 3	Date: 05/7/14	
Direction Photo Taken: South		
Description: View looking south of the concrete ramp, docks, light pedestals, and mooring piles.		

Photo No. 4	Date: 05/7/14
Direction Photo Taken: North	
Description: View at the end of the concrete ramp of the docks, light pedestals, and mooring piles.	



Facility Name: United States Coast Guard	Site Location: MWR Boat Ramp – USCG Base Elizabeth City, NC	Project No. 60321114
--	---	--------------------------------

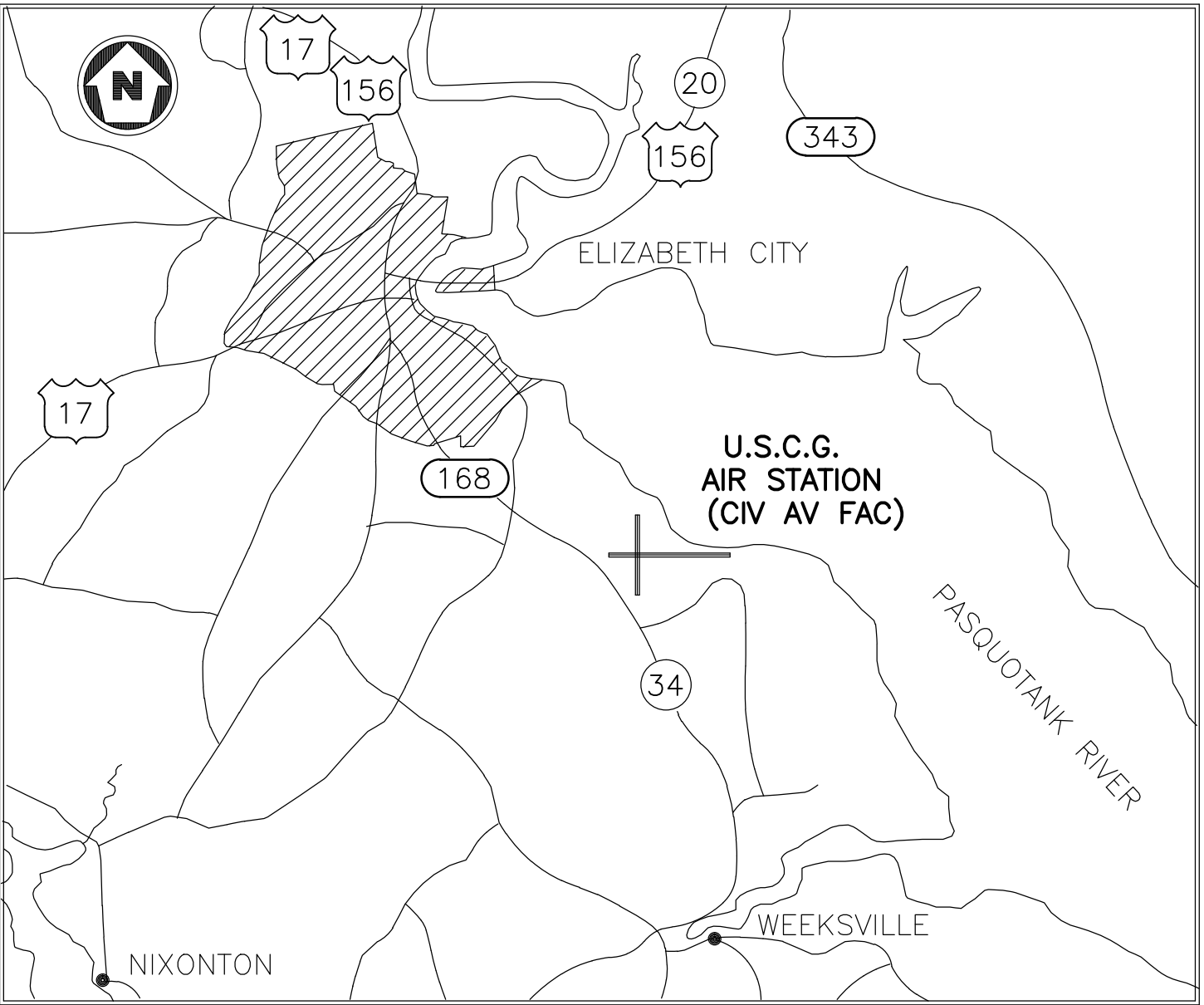
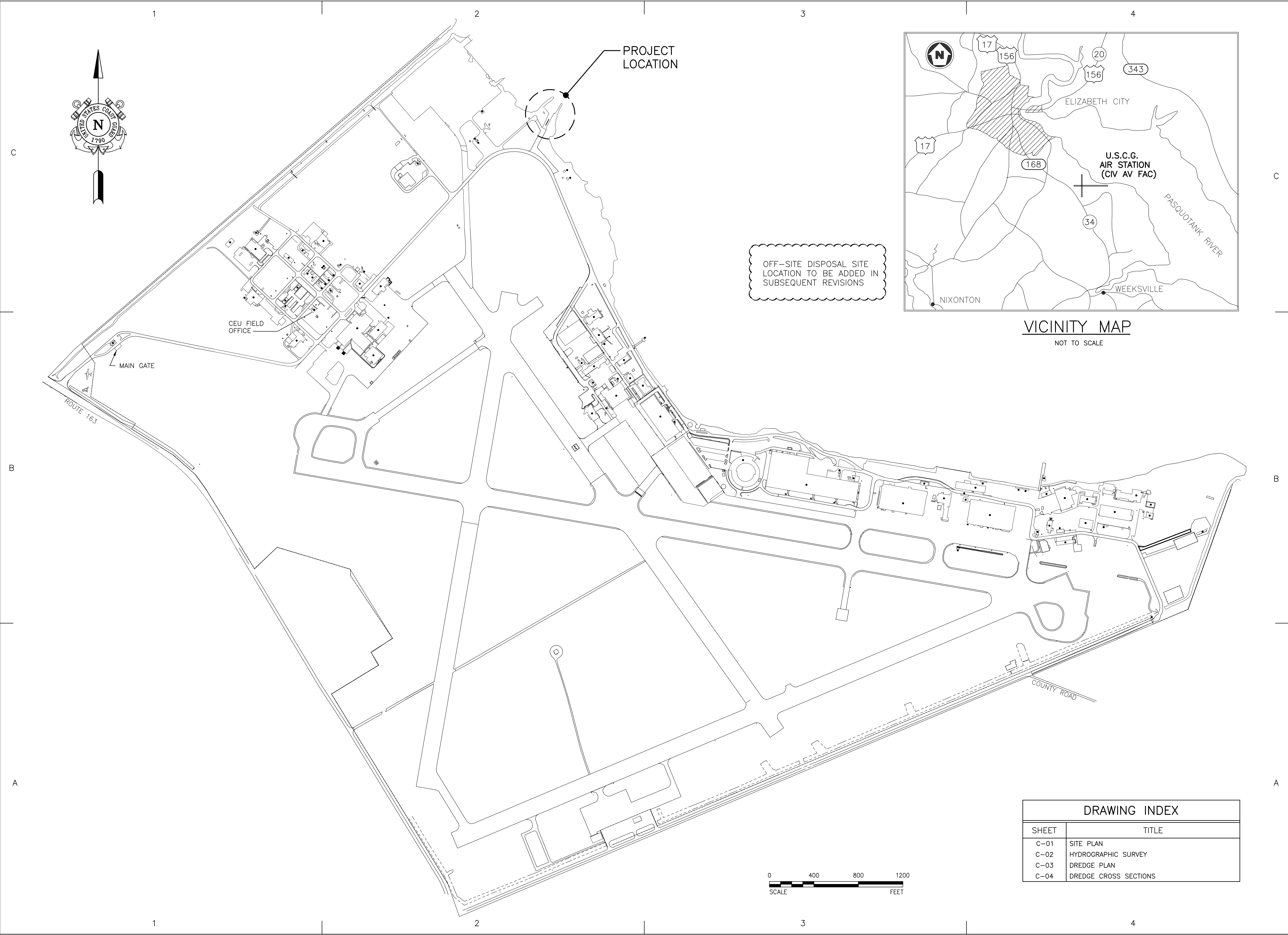
Photo No. 5	Date: 05/7/14
Direction Photo Taken: South-Southeast	
Description: View of the man-made beach located southeast of the MWR Boat Ramp.	

A wide-angle photograph taken from a slightly elevated position, looking down at a man-made beach. The foreground is dominated by dark, turbulent water with white foam from a boat's wake. A concrete structure, likely part of a boat ramp, extends from the right side into the water. The beach itself is a light-colored sand area. In the middle ground, there is a small, single-story wooden building with a brown roof. To the right of this building is a playground with yellow slides and a picnic shelter with a brown roof. Further back, there are more trees and a clear blue sky. The overall scene is bright and sunny.

Photo No. 6	Date: 05/7/14	
Direction Photo Taken: North		
Description: View of the navigation aids at entry to the protected harbor area. Note two timbers low to water surface in the entry-way.		

ATTACHMENT (D)

DREDGE SITE – 35% PLAN



VICINITY MAP

NOT TO SCALE

DRAWING INDEX

SHEET	TITLE
C-01	SITE PLAN
C-02	HYDROGRAPHIC SURVEY
C-03	DREDGE PLAN
C-04	DREDGE CROSS SECTIONS

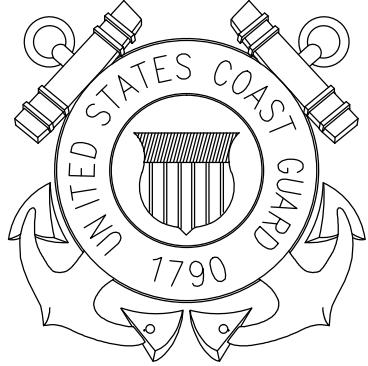
USCG CEU
CLEVELAND, OHIO
(216) 902-6200

CONSULTANTS

AECOM

8320 Guilford Road, Suite L
Columbia, MD 21046

U. S. COAST GUARD
CIVIL ENGINEERING UNIT
CLEVELAND



USCG. CEU CLEVELAND
1240 EAST 9TH STREET
CLEVELAND, OH 44199-2060

ISSUE		
MARK	DATE	DESCRIPTION

A/E PROJECT NO: 60321114
CAD FILE NAME: C8257C01.DWG
DESIGNED BY: SJR
DRAWN BY: MJC
EDITED BY: MJC
CHECKED BY: TAP

SCALE: AS SHOWN PLOT SCALE: 1:1

DREDGING AT MWR BOAT RAMP
CG BASE ELIZABETH CITY
ELIZABETH CITY
MWR BOAT RAMP
CIVIL
SITE PLAN

NC

REVIEWED BY: J.W. NICKS PROJECT ENG.	REVIEWED BY: G.P. LODGE BRANCH CHIEF	REVIEWED BY: G.S. PLACZEK TECH. DIRECTOR
--	--	--

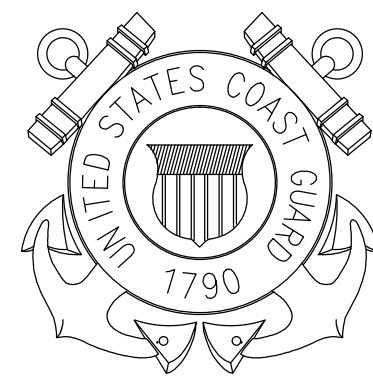
P.R. CARROLL, CDR. APPROVING OFFICER	DATE
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PROJECT NUMBER	DRAWING NUMBER
3756419	8257-D
DISCIPLINE/SHT NO	SHEET 1 OF 4
C-01	

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8320 Guilford Road, Suite 100
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CLEVELAND



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CLEVELAND, OH 44199-2060

ISSUE

MARK	DATE	DESCRIPTION

A/E PROJECT NO: 60321114

CAD FILE NAME: C8257C03.DWG

DESIGNED BY:

DRAWN BY: MJC

EDITED BY:

CHECKED BY: _____

SCALE: AS SHOWN

PLOT SCALE: 1:1

SHEET TITLE

DREDGING AT MWR BOAT RAMP
CG BASE ELIZABETH CITY
ELIZABETH CITY NC
MWR BOAT RAMP
CIVIL
DREDGE PLAN

REVIEWED BY:	REVIEWED BY:	REVIEWED BY:
J.W. NICKS	G.P. LODGE	G.S. PLACZEK
PROJECT ENG.	BRANCH CHIEF	TECH. DIRECTOR

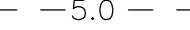



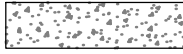
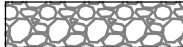
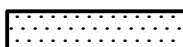
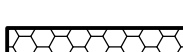

<u>P.R. CARROLL, CDR.</u>	<u> </u>
<u>APPROVING OFFICER</u>	<u>DATE</u>

PROJECT NUMBER	DRAWING NUMBER

3756419	8257-D
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DISCIPLINE/SHT NO	
C-03	SHEET 3 OF 4

LEGEND:

- | | |
|---|--|
|  | EXISTING CONTOUR (5' INTERVAL) |
|  | EXISTING CONTOUR (1' INTERVAL) |
|  | DREDGE COORDINATE POINT & ID |
| 11.3 | DEPTH MEASUREMENT (SOUNDING)
REFERENCED TO MEAN WATER LEVEL (MWL) |
| -1.0 | CONTOUR ELEVATION REFERENCED TO MWL |
|  | SPOT ELEVATION (ON PIER OR STRUCTURE) |
|  | EXISTING CONCRETE MATERIAL |
|  | EXISTING RIPRAP |
|  | PROPOSED DREDGING TO TARGET
DEPTH -7 MWL |
|  | PROPOSED OVER-DEPTH DREDGE
TO -8 MWL |
|  | PROPOSED DREDGE SIDE SLOPES
(SLOPES VARY - SEE PLAN) |

NOTES:

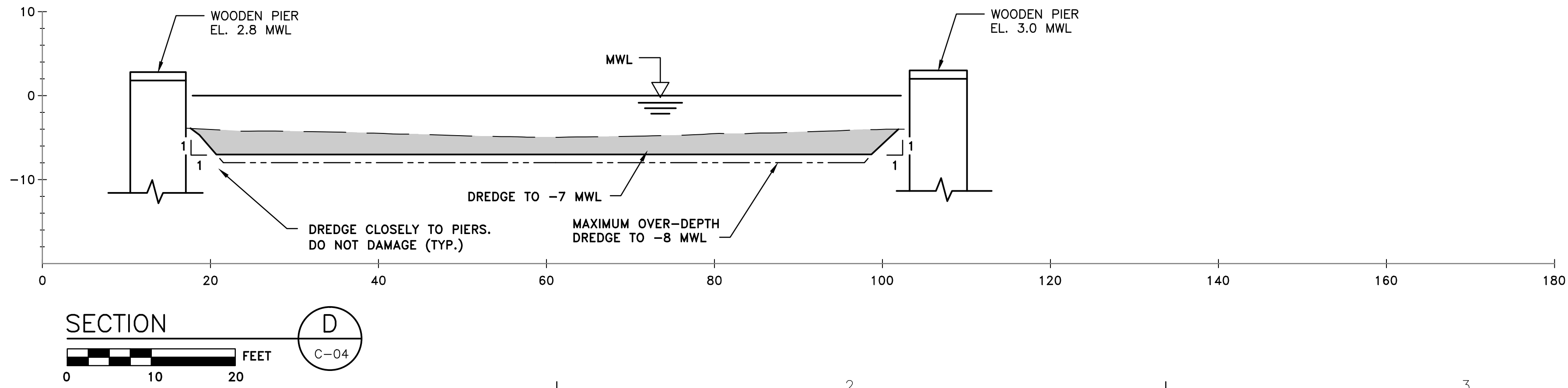
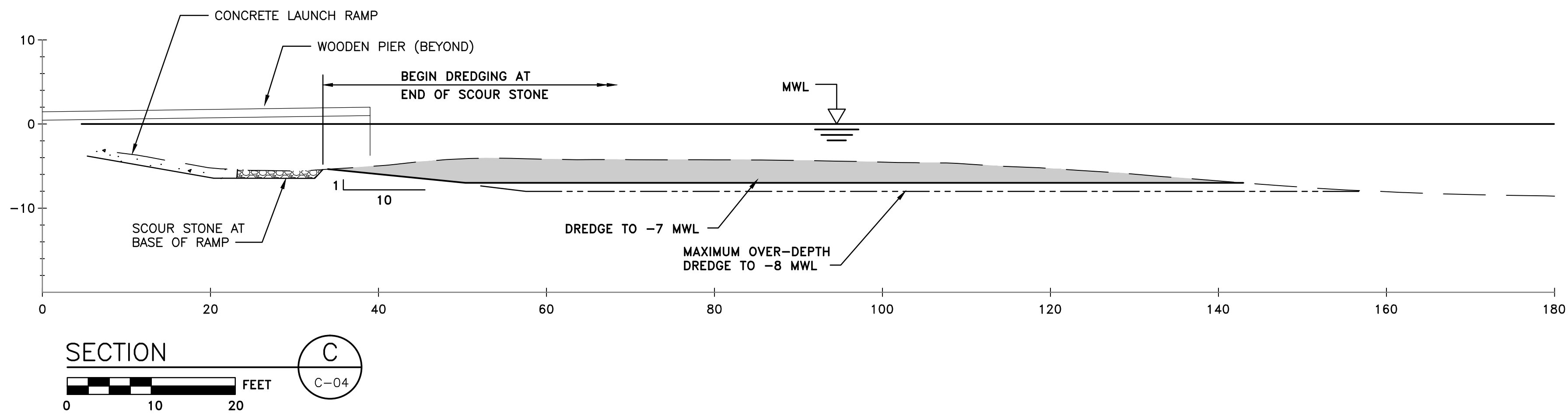
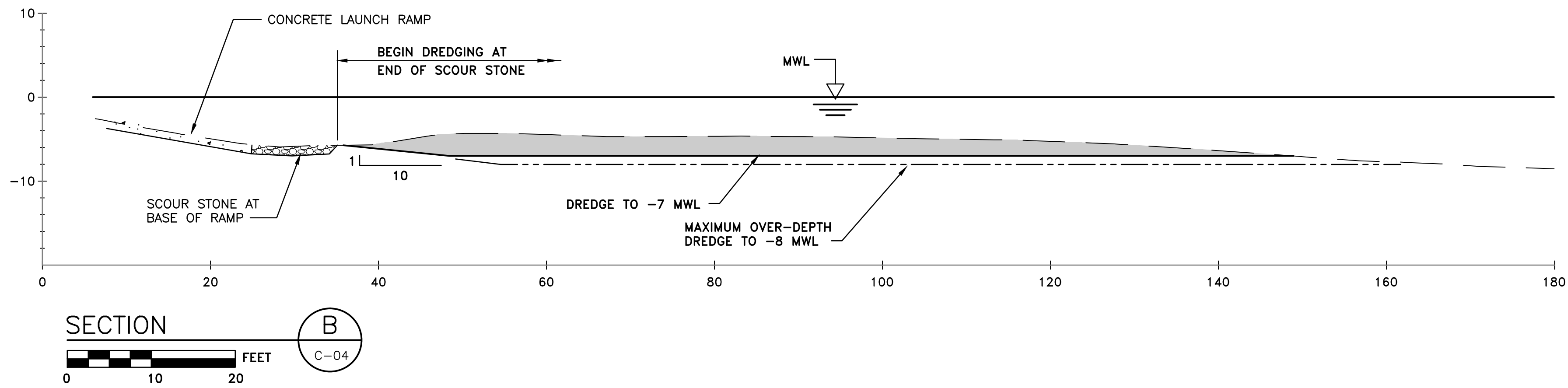
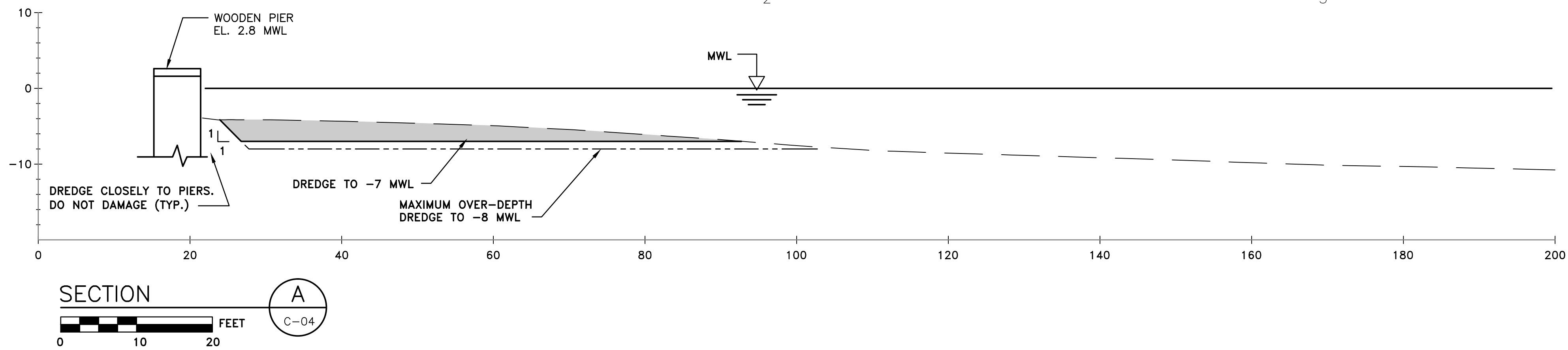
1. 0.0 MWL FOR PASQUOTANK RIVER = $-0.44'$ NAVD88
2. THE TARGET DREDGE DEPTH FOR THIS PROJECT IS -7 FT MWL. OVER-DEPTH DREDGE IN THESE AREAS SHALL NOT EXCEED -8 FT MWL.
3. CONTRACTOR SHALL USE METHODS AND EQUIPMENT THAT PROTECT THE PIER STRUCTURES FROM DAMAGE. CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE.
4. CONTRACTOR SHALL DREDGE AT A 10H:1V SLOPE AT THE LAUNCH RAMP STARTING AT THE END OF THE RIPRAP SCOUR PROTECTION.

DREDGE COORDINATE TABLE

DREDGE ID	NORTHING	EASTING
D1	929382.66	2831869.00
D2	929337.45	2831913.52

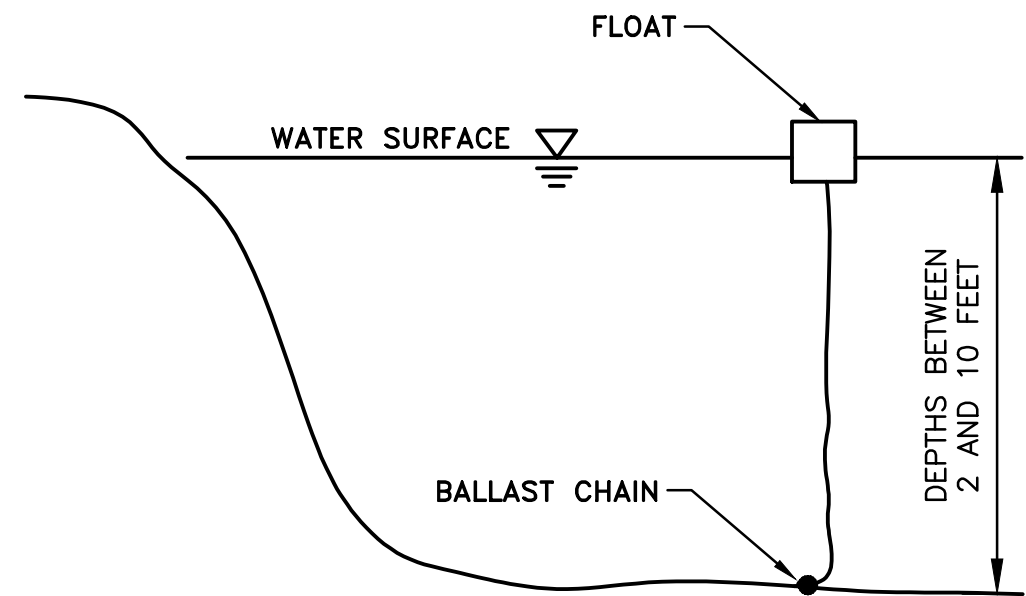
- COORDINATES ARE IN NORTH CAROLINA STATE PLANE GRID COORDINATE SYSTEM, FEET (NAD83).





LEGEND

- EXISTING TOP OF SEDIMENT
- PROPOSED BASE DREDGE ELEVATION
- - - MAXIMUM OVER-DEPTH DREDGE

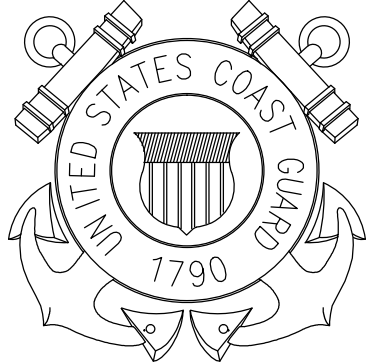


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(216) 902-6200

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U. S. COAST GUARD
CIVIL ENGINEERING UNIT
CLEVELAND



USCG. CEU CLEVELAND
1240 EAST 9TH STREET
CLEVELAND, OH 44199-2060

ISSUE		
MARK	DATE	DESCRIPTION

A/E PROJECT NO: 60321114
CAD FILE NAME: C8257C04.DWG
DESIGNED BY: SJR
DRAWN BY: MJC
EDITED BY: MJC
CHECKED BY: TAP

SCALE: AS SHOWN PLOT SCALE: 1:1

SHEET TITLE

DREDGING AT MWR BOAT RAMP
CG BASE ELIZABETH CITY
ELIZABETH CITY NC
MWR BOAT RAMP
CIVIL
DREDGE CROSS SECTIONS

REVIEWED BY:	REVIEWED BY:	REVIEWED BY:
J.W. NICKS	G.P. LODGE	G.S. PLACZEK
PROJECT ENG.	BRANCH CHIEF	TECH. DIRECTOR

P.R. CARROLL, CDR.	DATE
APPROVING OFFICER	

PROJECT NUMBER	DRAWING NUMBER
3756419	8257-D
DISCIPLINE/SHT NO	SHEET 4 OF 4

C-04

ATTACHMENT (E)

REFERENCES CITED

References Cited

- National Oceanic and Atmospheric Administration, Office of Response and Restoration, Environmental Sensitivity Index Maps, 2011. Accessed online September 2014 at <http://response.restoration.noaa.gov/esi>
- North Carolina Division of Marine Fisheries, Anadromous Fish Spawning Areas Maps, 2014. Accessed online October 2014 at <http://portal.ncdenr.org/web/mf/afsa-maps>
- North Carolina Division of Marine Fisheries, Fishing Nursery Areas, 2014. Accessed online October 2014 at <http://portal.ncdenr.org/web/mf/primary-nursery-areas>
- North Carolina Division of Marine Fisheries, Mapped Fish Habitats in Coastal North Carolina, 2014. Accessed online October 2014 at <http://portal.ncdenr.org/web/mf/58>
- North Carolina OneMap, Geospatial Portal, 2014. Accessed online September 2014 at <http://data.nconemap.com/geoportal/catalog/main/home.page>
- South Atlantic Fishery Management Council, Online Mapping, Essential Fish Habitat, 2014. Accessed online October 2014 at <http://www.safmc.net/ecosystem-management/mapping-and-gis-data>



☒ North Carolina Wildlife Resources Commission ☒

Gordon Myers, Executive Director

Ms. Jennifer Jones
AECOM
701 Corporate Center Drive, Suite 475
Raleigh, NC 27607

Subject: USCG Base Elizabeth City – Dredging at MWR Boat Ramp, Elizabeth City, NC

Dear Ms. Jones:

The North Carolina Wildlife Resources Commission (NCWRC) received your letter regarding the above subject and request for any additional information as it related to aquatic and terrestrial wildlife resources in the project area.

The project is located within the Pasquotank River, along the south shore approximately 3.5 miles southeast of Elizabeth City, NC. This area of the Pasquotank River is designated an Anadromous Fish Spawning Area (AFSA). Due to the importance of this area to anadromous species, a February 15 – June 30 moratorium may be requested for in-water work to minimize noise, vibration, and elevated turbidity levels during this period of time.

The NCWRC does not generally object to maintenance dredging activities, such as within boat ramps, when the project is maintained to the original permit dimensions. However, if this is unknown, dredging to depths deeper than connecting water depths should be avoided. This minimizes impacts to water quality and is consistent with NC Division of Coastal Management rule.

If you have additional questions or would like to discuss this project in more detail, please contact me at (252)948-3916 or at maria.dunn@ncwildlife.org

Sincerely,

Maria T. Dunn
Coastal Coordinator

**U.S. Department of
Homeland Security**

**United States
Coast Guard**



Commanding Officer
United States Coast Guard
Civil Engineering Unit Cleveland

1240 East Ninth Street
Room 2179
Cleveland Ohio 44199-2060
Staff Symbol: ER
Phone: (216) 902-6219
Fax: (216) 902-6277
Email: Gregory.O.Carpenter@uscg.mil

11000

National Oceanic and Atmospheric Administration
National Marine Fisheries Service
Southeast Regional Office

Submitted via e-mail to: nmfs.ser.esa.consultations@noaa.gov

SUBJECT: Integrated EFH and ESA Section 7 Consultation Request
U.S. Coast Guard Base Elizabeth City
Proposed Dredging at MWR Boat Ramp
Elizabeth City, Pasquotank County, North Carolina

Dear Sir/Madam:

Pursuant to Section 7 of the Endangered Species Act (ESA) of 1973 and the Magnuson-Stevens Fishery Conservation and Management Act (MSA) Sections 305(b)(2), the United States Coast Guard (USCG) is submitting this interagency consultation request. The USCG proposes to complete maintenance dredging of the boat basin and moorings at the Morale, Welfare and Recreation (MWR) Boat Ramp at the USCG Base Elizabeth City located at 1664 Weeksville Road, Elizabeth City, Pasquotank County, North Carolina. The geographic location of the MWR Boat Ramp is latitude 36° 16' 14.72" north and longitude 76° 10' 40.31" west.

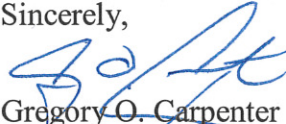
The USCG, Civil Engineering Unit (CEU) Cleveland, Environmental Section conducted an effects determination of the proposed project and determined that the proposed action may affect, but is not likely to adversely affect, Endangered Species Act (ESA) listed species and Essential Fish Habitat (EFH).

This correspondence provides your office with a completed National Marine Fisheries Service (NMFS) ESA Section 7 Checklist as Attachment A. The Section 7 Checklist also includes information pertinent to the EFH Assessment. The proposed project Site Location Map and other supporting figures are presented as Attachments B, C and D. Attachment E contains the Dredge Plan. Attachment F contains a list of references reviewed in the effects determination.

SUBJECT: Integrated EFH and ESA Section 7 Consultation Request

The USCG requests your written concurrence in response to our informal consultation. If you have any questions, please contact Ms. Leilani Woods of my staff at (252) 335-6847. Alternatively, you may contact Ms. Jennifer Jones with our environmental firm (AECOM) at (919) 239-7150.

Sincerely,



Gregory O. Carpenter
Chief, Environmental Compliance
By direction of the Commanding Officer

Attachments:

- (A) NMFS Endangered Species Act Section 7 Checklist
- (B) Dredge Site - Location Map
- (C) Dredge Site - Aerial Photograph
- (D) Dredge Site - Ground-Level Photographs
- (E) Dredge Site - 35% Plan
- (F) References Cited

ATTACHMENT (A)

NMFS Endangered Species Act Section 7 Checklist

NMFS Endangered Species Act Section 7 Checklist

A) Project Identification

Lead Action Agency:

United States Coast Guard

Agency Contact: (Phone, Email)

Leilani Woods / Leilani.L.Woods@uscg.mil / (252) 335-6847

Applicant Name/ Contact: (Phone, Email)

AECOM, Jennifer Jones / Jennifer.Jones@aecom.com / (919) 239-7150

Project Name & ID #:

Dredging at MWR Boat Ramp / USCG Project# 3756419

B) Project Location

1. Address and description of property (public, residential, commercial, industrial, etc.):

1664 Weeksville Road, Elizabeth City, NC/USCG Base Elizabeth City (Government property). Dredging will occur at the USCG's boat ramp located within the Pasquotank River. The boat ramp is located at the north end of the base, along the south shore of the river.

2. Latitude & Longitude:

i. Decimal Degrees and Datum (e.g., 27.71622° N, 80.25174° W [NAD83])

ii. Online conversion: <http://transition.fcc.gov/mb/audio/bickel/DDDMSS-decimal.html>

36.270836°N, 76.177862°W [NAD83]

3. Waterbody:

i. Name of the body of water on which the project is located (St. Johns River, Tampa Bay, Suwannee River, etc.)

ii. If riverine or estuarine, approximate navigable distance from marine environment (e.g., Atlantic, Gulf of Mexico)

The proposed project will occur within the Pasquotank River, approximately 10 nautical miles from Albemarle Sound and approximately 30 nautical miles from the Atlantic Ocean.

C) Project Site Description

1. Existing Structures: (Describe current structures in project area.)

i. Marina, seawall, riprap, dock, etc.

ii. Number of slips, size (area of overwater structures), linear footage, location, orientation, etc.

The MWR Boat Ramp consists of a two-lane concrete ramp with a concrete pavement approach and a launch dock centered in the boat ramp. Rubble mound stone breakwaters project approximately 100 to 125 feet from the shoreline on both sides of the launch ramp to form a protected harbor area. Timber docks line both sides of the boat basin. Mooring piles are in place along the west timber dock.

2. Existing Conditions: (Describe the project area.)

i. Substrate type, water quality, depth, current, etc.

i. The substrate within the project area consists primarily of poorly graded sand with some silty sand (U.S.C.S. class is SP and SP-SM).

ii. Water quality parameters measured on 5/14/14: temperature 26.15 degrees Celsius; pH 6.95; conductivity 1.208 millisiemens per centimeter; dissolved oxygen 6.25 milligrams per liter, oxidation reduction potential 97.3 millivolts.

iii. Water depth at MWR Boat Ramp based on hydrographic survey performed on 5/7/14 ranged from 3.9 to 8.0 feet within the proposed project area.

iv. The project site is located within the estuarine waters of the Pasquotank River. The portion of the Pasquotank River where the project site is located is classified as "SB", which is described as "Primary Recreation, Salt Water".

v. Water depth in the boat basin and moorings is often reduced by heavy, sustained winds from the north, which push water from the boat basin and down the Pasquotank River. This wind effect decreases the depth of the water in the boat basin and moorings.

3. Seagrasses & Other Marine Vegetation:

- i. If a benthic survey was conducted , provide date of survey and a copy of the report.
- ii. Species area of coverage estimates and density of species coverage (percentage) estimates.
- iii. Location relative to proposed structures. Provide detailed sketch of action area and location of all marine vegetation.

Not applicable. Also, the project site is not located in the vicinity of a designated submerged aquatic vegetation area.

4. Mangroves:

- i. Species (red, black, or white)
- ii. Area (square footage and linear footage). Provide detailed sketch of action area and location of mangroves.

Not applicable.

5. Corals:

- i. Species area of coverage estimates (percentage) and density of species estimates.
- ii. Location relative to proposed structures. Provide detailed sketch of action area and location of corals.

Not applicable.

D) Project Description and Construction Methods

1. Project: (Please provide detail.)

The purpose of this project is to dredge the boat basin and moorings to a required depth below mean water level at the MWR Boat Ramp due to changing conditions at the site. Water depth in the boat basin and moorings is reduced due to natural sediment deposition and wind-induced water level fluctuations, which encumber the USCG's ability to utilize the existing boat ramp for both operational missions and recreational purposes.

The MWR Boat Ramp is a previously dug man-made basin and the proposed dredging will occur entirely within the previously disturbed footprint of the boat basin and moorings. However, the USCG has no record of the as-built drawings indicating the original dredge depth. While the dredging required for the original construction of the MWR Boat Ramp likely disturbed the sediments to a depth proximate to the proposed dredge depth, the exact depth of the original dredging is not documented. Therefore, the USCG has elected to conduct an Environmental Assessment (EA) to address National Environmental Policy Act of 1969 requirements.

2. Methods:

- i. Construction methodology (Please provide detail.)
- ii. Demolition/removal of existing structures/debris
- iii. Location of work (barge, upland, or both)

The project involves mechanical dredging of the boat basin and moorings to a maximum depth of 8 feet below mean water level. Mean water level corresponds to elevation -0.44 North American Vertical Datum 1988 (NAVD88). Therefore, dredging will proceed to -8.44 feet NAVD88. The calculated dredge volume for the MWR Boat Ramp is 755 cubic yards of sediment. The proposed dredging project will only temporarily impact open waters (an area totaling approximately 0.18 acres) and will occur entirely within the existing boat basin (limited to an area approximately 150 feet from the shoreline).

The dredging will be performed from a floating plant and not from land based operations. During the proposed dredging activities, the dredge area will be closed with an impermeable floating turbidity curtain with a weighted bottom edge. The dredge material will be deposited on barges, dewatered on the barges within the turbidity curtain, transported by barge to an unloading site, offloaded to trucks, and transported by truck to a licensed landfill for disposal.

3. Overwater Structures:

i. Is the proposed use of this structure for a docking facility or an observation platform?

ii. If no, is this a fishing pier? (public or private)

(a) If a fishing pier, how many people are expected to fish per day? (Use box below.)

(b) If a fishing pier, how do you plan to address hook and line captures? (Use box below.)

Section D, Question 3 is not applicable because the proposed project does not involve development or construction.

iii. Will Dock Guidelines - in Florida be used?

Dock Guidelines - Construction Guidelines in Florida for Minor Piling-Supported Structures Constructed in or over Submerged Aquatic Vegetation (SAV), Marsh or Mangrove Habitat. U.S. Army Corps of Engineers/National Marine Fisheries Service August, 2001

iv. If in Johnson's range, will Dock Construction Key regarding Johnson's seagrass be used?

Dock Key - Key for Construction Conditions for Docks or Other Minor Structures Constructed in or Over Johnson's Seagrass (*Halophila johnsonii*). National Marine Fisheries Service/U.S. Army Corps of Engineers, October 2002

(a) Grated (yes/no/NA)

v. Type of decking

1. 43% open space (yes/no/NA)

(b) Wooden planks or composite planks

1. Proposed spacing between boards (0.50-inch, 0.75-inch, etc.)

vi. Height above Mean High Water (MHW) elevation

vii. Directional orientation of main axis of dock

viii. Overwater area (calculate square footage)

ix. Will Sea Turtle and Smalltooth Sawfish Construction Conditions, dated March 23, 2006 be used? (yes/no)

4. Pilings & Sheetpiles

i. Construction methodology (pile driving, vibratory hammer, jetting, etc.)

ii. Provide piling size, material, and number of pilings (mandatory)

iii. Have potential impacts to species been adequately addressed (noise impacts, marine vegetation impacts, etc.)?

Section D, Question 4 is not applicable because the proposed project does not involve development or construction.

5. Boat Slips

i. Number and size of new slips, change from existing

ii. High-and-dry boat storage: vessel storage capacity

iii. Estimated shadow effect of the boat (square footage of shaded area beneath boat)

Section D, Question 5 is not applicable because the proposed project does not involve development or construction.

6. Boat Ramp

i. Number of ramps and size of ramps

ii. Number of vessels that can be moored (e.g., staging area, shoreline docks for loading/unloading, etc.)

iii. Trailer parking lot capacity, and if this number changes from what is currently available at the project.

Section D, Question 6 is not applicable because the proposed project does not involve development or construction.

7. Shoreline Armoring: Seawalls, jetties, riprap, groins, breakwaters, etc.

i. Project description, linear footage, square footage, material, etc. Provide detailed sketch of action area and location of structure.

Section D, Question 7 is not applicable because the proposed project does not involve development or construction.

8. Dredging

- i. Dredge type (hopper, cutterhead, clamshell, etc.)
- ii. Depth of cut
- iii. Area (square feet) to be dredged
- iv. Volume of material (cubic yards)
- v. Spoil deposition plans: Where is dredged material being disposed of? Location of disposal area (upland/openwater/beneficial use site); sediment type at disposal area; thickness of fill placement; etc.
- vi. Hydrodynamic description (average current speed/direction, etc.)

i. The project involves mechanical dredging (grab/clamshell) of the boat basin and moorings to a maximum depth of 8 feet below mean water level (includes 1 foot over dredge depth). Mean water level corresponds to elevation -0.44 North American Vertical Datum 1988 (NAVD88). Therefore, dredging will proceed to -8.44 feet NAVD88.

ii. The calculated dredge volume for the MWR Boat Ramp is 755 cubic yards of sediment. The proposed dredging project will only temporarily impact open waters (an area totaling approximately 7,945 square feet) and will occur entirely within the existing boat basin (limited to an area approximately 150 feet from the shoreline).

iii. The dredging will be performed from a floating plant and not from land based operations. During the proposed dredging activities, the dredge area will be closed with an impermeable floating turbidity curtain with a weighted bottom edge. The dredge material will be deposited on barges, dewatered on the barges within the turbidity curtain, transported by barge to an unloading site, offloaded to trucks, and transported by truck to the East Carolina Regional Landfill in Aulander, North Carolina for disposal. Formal approval for disposal of the dredge material at the MWR Boat Ramp has been received from the East Carolina Regional Landfill.

iv. The sediment within the project area consists primarily of poorly graded sand with some silty sand (U.S.C.S. class is SP and SP-SM). The grain size analysis of sediment collected within the project area indicated that the sediment material consisted of predominately poorly graded sand (95.5 to 93.2 percent sand) with 4.5 to 6.8 percent fines (passing a # 200 sieve). For disposal purposes, the sediment was tested for a variety of parameters and the results were either below the laboratory's reportable limits or the North Carolina "Contained-Out" Levels for Unrestricted Use. The sediment material was characterized as non-hazardous.

v. Site-specific hydrodynamic data is not available for the project site. An Ecological Assessment of the Nearshore Habitat at the Camden Reverse Osmosis - Water Treatment Plant (RO-WTP), located in Chantilly Bay in the Pasquotank River and approximately 1.8 nautical miles from the MWR Boat Ramp, was reviewed. The hydrodynamic description was taken from 2005 and 2006 data collected at a sample site located approximately 1,200 feet offshore from the RO-WTP. Results of the study indicate that the average surface water speed was highly variable and ranged from 100 meters per hour to 1.1 kilometers per hour depending on the time of the year. Prevalent surface water flow direction was to the SE depending on the time of the year.

9. Blasting

- i. Explosive weights
- ii. Blasting plan

Not applicable.

10. Artificial Reefs

Please refer to the Section 7 Checklist procedures for directions on how to complete this question.

Not applicable.

11. Construction Schedule

- i. In-water work
- ii. Number of days/weeks/months; daytime/24-hour; seasonal restriction; etc.

The exact dredging schedule has not been determined yet; dredge work will be submitted for bid. Anticipated work hours are daytime only. To avoid potentially adverse impacts on life cycles of anadromous fish, the proposed project is expected to occur outside of the anadromous fish spawning moratorium (February 15 to June 30).

12. Mitigation/ Protective Measures:

How is mitigation or other protective measures being incorporated in this project, if any?

Measures will be taken to avoid or minimize adverse impacts to natural and biological resources. The proposed dredging project will only temporarily impact open waters (an area totaling approximately 7,945 square feet) and will occur entirely within the existing boat basin (limited to an area approximately 150 feet from the shoreline). The dredging will be performed from a floating barge and not from land based operations. Prior to commencing dredging operations, the proposed area for dredging will be closed with an impermeable floating turbidity curtain. The turbidity curtain will remain in place in accordance with the permit conditions or for a period of 48 hours following completion of the dredging, whichever occurs longer. Additionally, the proposed dredging is expected to occur outside of the anadromous fish spawning moratorium.

E) Effects of the Project

1. Listed Species and Critical Habitat within the Action Area

- Please indicate which listed species and critical habitats are within the Action Area.
- For information on species and critical habitat under NMFS jurisdiction that may be present within the action area, visit "[Species and Critical Habitat Found in the Southeast Region](#)" page. If you are uncertain of which areas contain critical habitat, please visit the [Maps and GIS Data](#) page.

Species	Effect Determination	Critical Habitat	Effect Determination
Shortnose Sturgeon	NLAA	Not in Critical Habitat	No Effect
Atlantic Sturgeon	NLAA	Not in Critical Habitat	No Effect
Select One	Select One	Select One	Select One
Select One	Select One	Select One	Select One
Select One	Select One	Select One	Select One
Select One	Select One	Select One	Select One
Select One	Select One	Select One	Select One
Select One	Select One	Select One	Select One
Select One	Select One	Select One	Select One
Select One	Select One	Select One	Select One
Select One	Select One	Select One	Select One
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Select One	Select One	Select One	Select One
Select One	Select One	Select One	Select One
Select One	Select One	Select One	Select One
Select One	Select One	Select One	Select One
Select One	Select One	Select One	Select One
Select One	Select One	Select One	Select One

2. Effects to Species

- Explain potential route of effects to each species and critical habitat checked above

i. Heavy equipment and construction activity noises will result from dredging and transport of the dredged material to the landfill by barge and truck. The noise from heavy equipment and dredging activities will repel nearby fish (including Shortnose sturgeon and Atlantic sturgeon). Fish will likely relocate to a nearby habitat when the dredging activities begin, and will likely return after these activities are completed. Therefore, the fish will be affected by evidence of their avoidance; however, since the effects will be temporary and short-term and have no realistic potential to lead to harm or harassment of the fish, the effect is insignificant.

ii. The water quality may be affected by the dredging activities by temporarily increasing turbidity. However, prior to commencing dredging activities the dredge area will be enclosed with an impermeable floating turbidity curtain. The curtain will be of a full depth floating design with a weighted bottom edge. The dredge material will be dewatered on the barges within the turbidity curtain and the turbidity curtain will remain in place in accordance with the permit conditions or for a period of 48 hours following completion of the dredging, whichever occurs longer. Therefore, the water quality effects are expected to be temporary and minimal because the suspended particles will settle out within a short time frame without measurable effects on water quality and increases in turbidity will be abated by use of the turbidity curtain.

iii. Since the proposed dredging will occur entirely within the previously disturbed footprint of the boat basin and moorings and the dredge depth is expected to be proximate to the original dredge depth, the project is not expected to have an adverse effect on any unique habitat conditions that would impact the continued survival of federally managed species. Based on the relatively small dredge area (an area totaling approximately 7,945 square feet) and volume of material to be removed (755 cubic yards), and that the dredging will occur within a previously disturbed footprint, effects to the aquatic environment are expected to be insignificant.

3. Effects to Critical Habitat:

- i. Identify which essential feature(s) are present, if they will be impacted, and how they will be impacted
- ii. Size of area affected (square footage) - Mangroves (linear footage of shoreline)
- iii. How will the habitat be changed/altered as a result of the action

Not applicable. No designated critical habitat exists within the proposed project area.

Revised on: September 25, 2014

ATTACHMENT (B)

DREDGE SITE - LOCATION MAP



Map Location



Site Location Map

MWR Boat Ramp
USCG Base Elizabeth City, North Carolina
Elizabeth City NC Topographic Quadrangle (1983)

0 1,000 2,000
Feet

June 2014

60321114

Figure 1

AECOM

AECOM Environment
701 Corporate Center Drive, Suite 475
Raleigh, NC 27607
Phone: (919) 854-6200
Fax: (919) 854-6259
Web: <http://www.aecom.com>

ATTACHMENT (C)

DREDGE SITE – AERIAL PHOTOGRAPH



Scale: Not To Scale

(Source: Google Earth© 2014)

Dredge Site - Aerial Photograph

US Coast Guard
Base Elizabeth City – MWR Boat Ramp
Elizabeth City, NC

AECOM

Attachment B

www.aecom.com

ATTACHMENT (D)

DREDGE SITE – GROUND-LEVEL PHOTOGRAPHS

Facility Name: United States Coast Guard	Site Location: MWR Boat Ramp – USCG Base Elizabeth City, NC	Project No. 60321114
--	---	--------------------------------

Photo No. 1	Date: 05/7/14	
Direction Photo Taken: North		
Description: View from the southern side of the shore (concrete pavement approach) of the concrete ramp, docks, navigation aids, and light pedestals.		

Photo No. 2	Date: 05/7/14	
Direction Photo Taken: Northwest		
Description: View from the concrete pavement approach along the southern shoreline.		

Facility Name: United States Coast Guard	Site Location: MWR Boat Ramp – USCG Base Elizabeth City, NC	Project No. 60321114
--	---	--------------------------------

Photo No. 3	Date: 05/7/14
Direction Photo Taken: South	
Description: View looking south of the concrete ramp, docks, light pedestals, and mooring piles.	

A wide-angle photograph taken from a wooden dock, looking south down a concrete boat ramp. The ramp extends from the foreground into the dark blue water. On the right side of the ramp, several wooden mooring piles are visible. In the background, a grassy area with some buildings and a small boat on the water can be seen under a clear blue sky.

Photo No. 4	Date: 05/7/14
Direction Photo Taken: North	
Description: View at the end of the concrete ramp of the docks, light pedestals, and mooring piles.	



Facility Name: United States Coast Guard	Site Location: MWR Boat Ramp – USCG Base Elizabeth City, NC	Project No. 60321114
--	---	--------------------------------

Photo No. 5	Date: 05/7/14
Direction Photo Taken: South-Southeast	
Description: View of the man-made beach located southeast of the MWR Boat Ramp.	

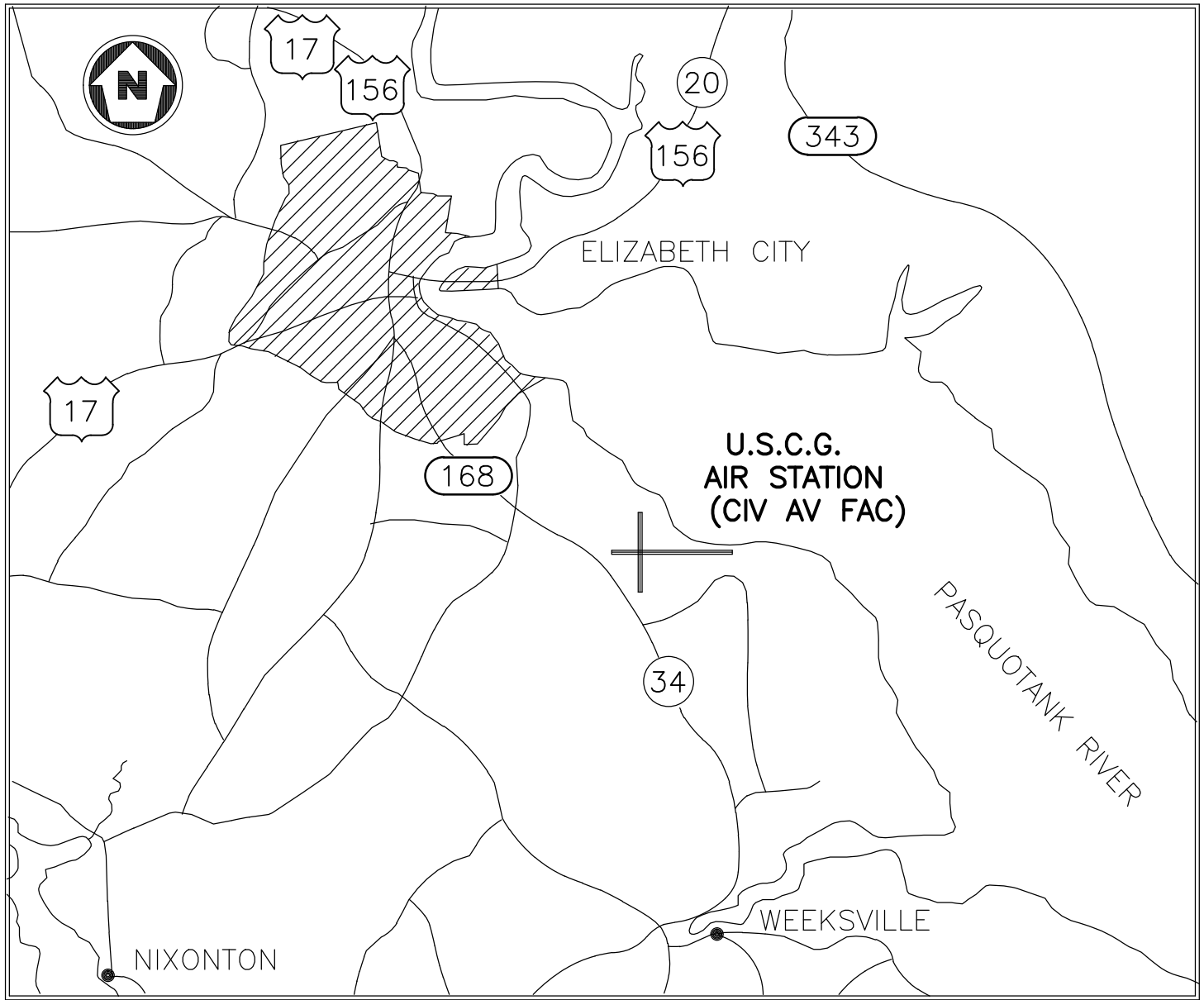
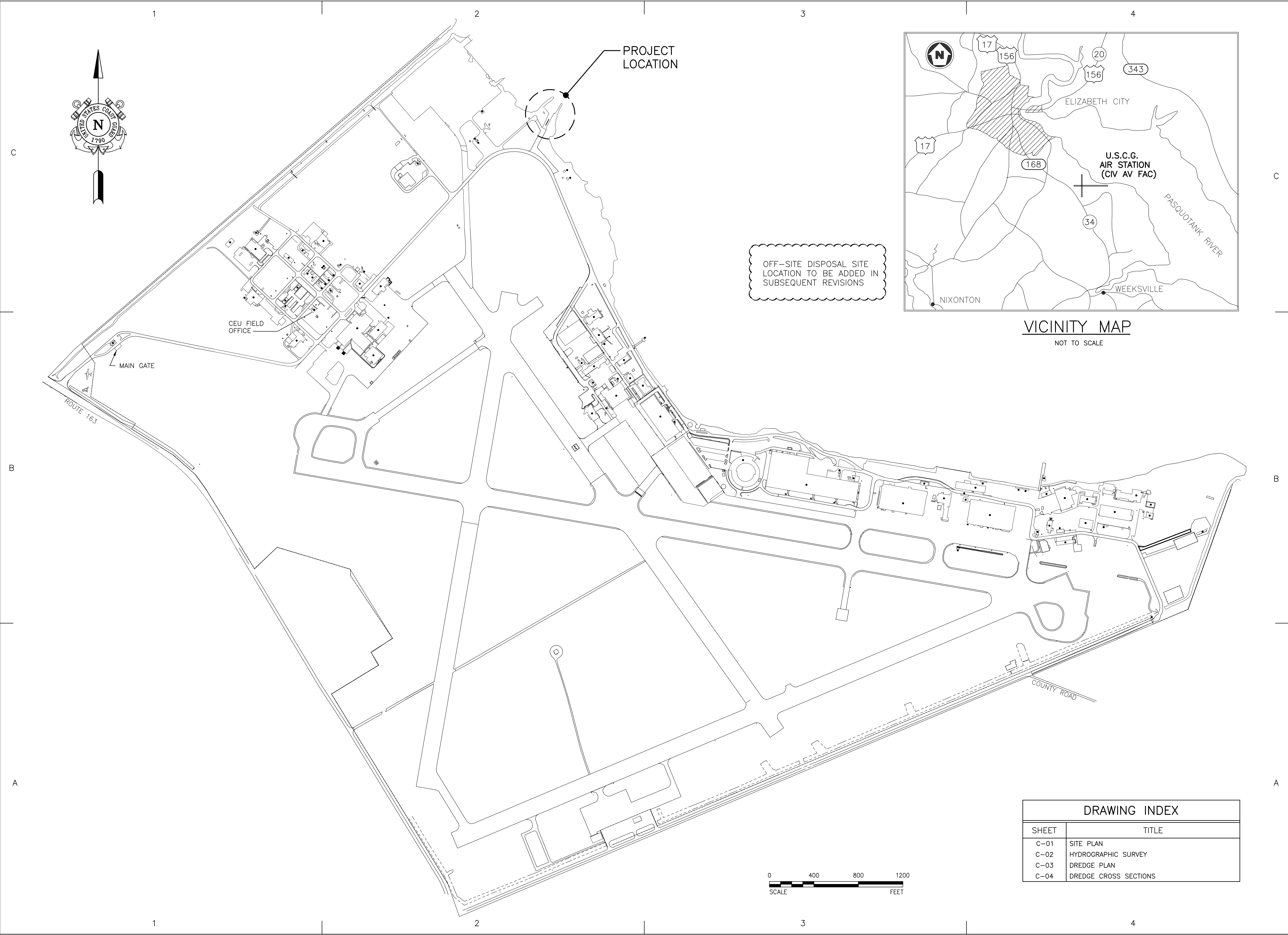


Photo No. 6	Date: 05/7/14
Direction Photo Taken: North	
Description: View of the navigation aids at entry to the protected harbor area. Note two timbers low to water surface in the entry-way.	

A photograph showing a harbor entrance. In the center, a small boat with a red flag and a person on board is moving towards the viewer. To the left, a wooden pier extends into the water. To the right, a green square navigation aid is visible on a post, and two dark timbers are partially submerged in the water. The water is dark blue, and the sky is clear. A rocky shoreline is visible in the bottom right corner.

ATTACHMENT (E)

DREDGE SITE – 35% PLAN



VICINITY MAP

NOT TO SCALE

DRAWING INDEX

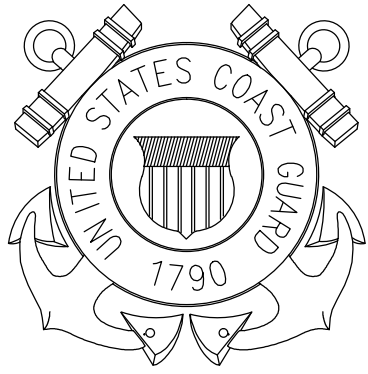
SHEET	TITLE
C-01	SITE PLAN
C-02	HYDROGRAPHIC SURVEY
C-03	DREDGE PLAN
C-04	DREDGE CROSS SECTIONS

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CLEVELAND, OH 44199-2060

ISSUE		
MARK	DATE	DESCRIPTION

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DESIGNED BY: SJR
DRAWN BY: MJC
EDITED BY: MJC
CHECKED BY: TAP

SCALE: AS SHOWN PLOT SCALE: 1:1

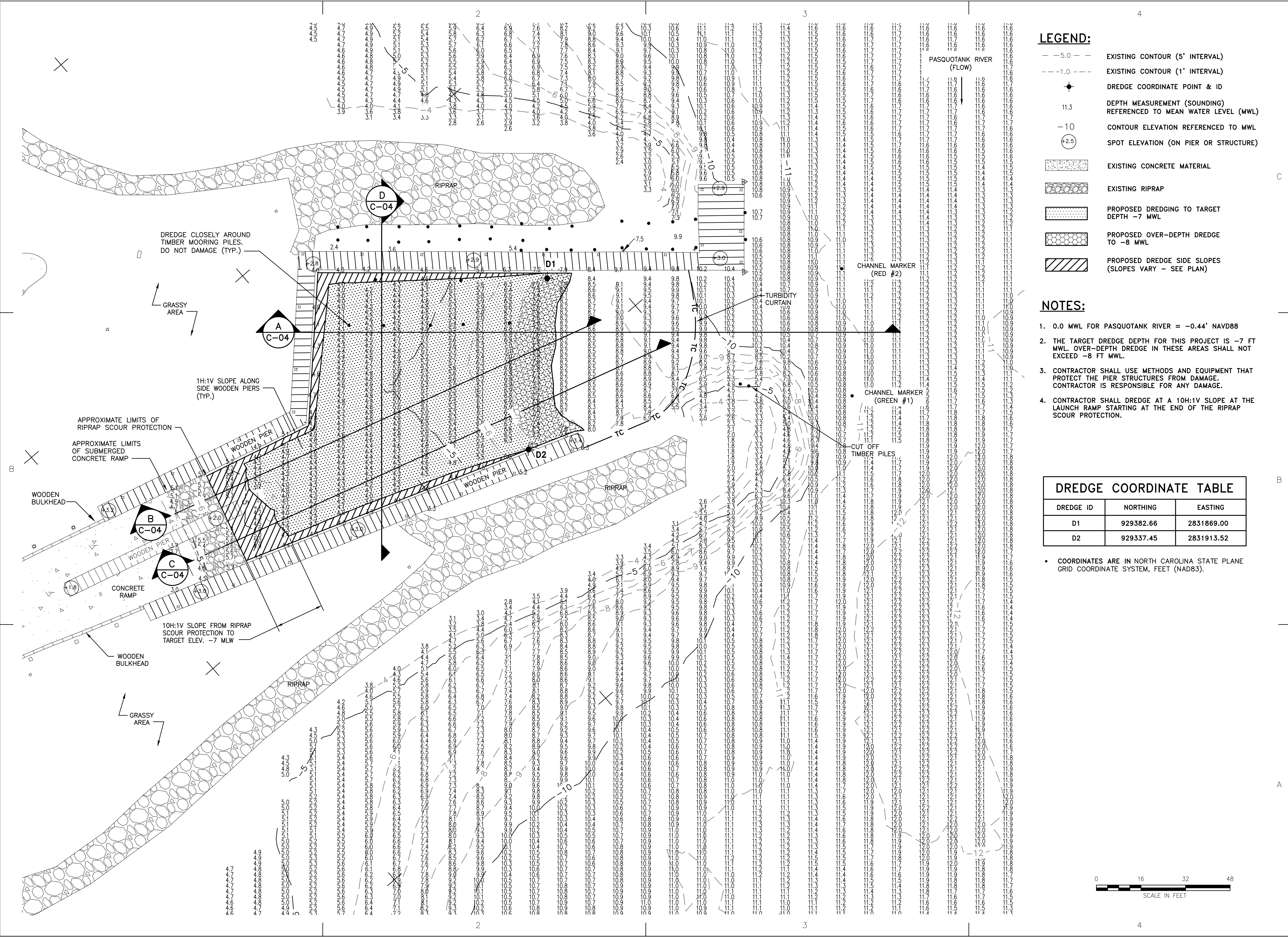
SHEET TITLE

DREDGING AT MWR BOAT RAMP
CG BASE ELIZABETH CITY
ELIZABETH CITY NC
MWR BOAT RAMP
CIVIL
SITE PLAN

REVIEWED BY: J.W. NICKS PROJECT ENG.	REVIEWED BY: G.P. LODGE BRANCH CHIEF	REVIEWED BY: G.S. PLACZEK TECH. DIRECTOR
--	--	--

P.R. CARROLL, CDR. APPROVING OFFICER	DATE
---	------

PROJECT NUMBER	DRAWING NUMBER
3756419	8257-D
DISCIPLINE/SHT NO C-01	SHEET 1 OF 4



LEGEND:

- EXISTING CONTOUR (5' INTERVAL)
- EXISTING CONTOUR (1' INTERVAL)
- DREDGE COORDINATE POINT & ID
- DEPTH MEASUREMENT (SOUNDING) REFERENCED TO MEAN WATER LEVEL (MWL)
- CONTOUR ELEVATION REFERENCED TO MWL
- SPOT ELEVATION (ON PIER OR STRUCTURE)
- EXISTING CONCRETE MATERIAL
- EXISTING RIPRAP
- PROPOSED DREDGING TO TARGET DEPTH -7 MWL
- PROPOSED OVER-DEPTH DREDGE TO -8 MWL
- PROPOSED DREDGE SIDE SLOPES (SLOPES VARY - SEE PLAN)

NOTES:

- 0.0 MWL FOR PASQUOTANK RIVER = -0.44' NAVD88
- THE TARGET DREDGE DEPTH FOR THIS PROJECT IS -7 FT MWL. OVER-DEPTH DREDGE IN THESE AREAS SHALL NOT EXCEED -8 FT MWL.
- CONTRACTOR SHALL USE METHODS AND EQUIPMENT THAT PROTECT THE PIER STRUCTURES FROM DAMAGE. CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE.
- CONTRACTOR SHALL DREDGE AT A 10H:1V SLOPE AT THE LAUNCH RAMP STARTING AT THE END OF THE RIPRAP SCOUR PROTECTION.

DREDGE COORDINATE TABLE		
DREDGE ID	NORTHING	EASTING
D1	929382.66	2831869.00
D2	929337.45	2831913.52

- COORDINATES ARE IN NORTH CAROLINA STATE PLANE GRID COORDINATE SYSTEM, FEET (NAD83).

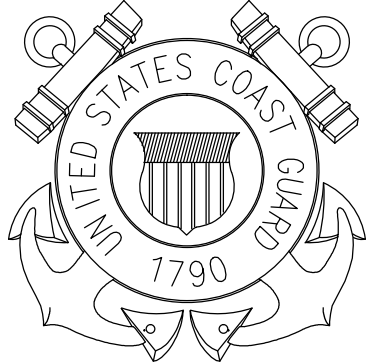


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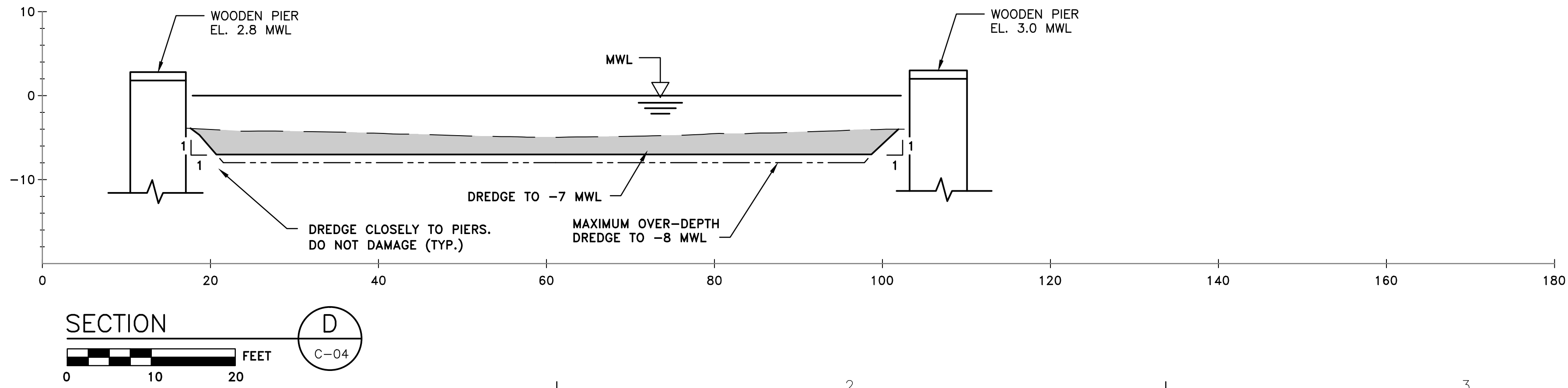
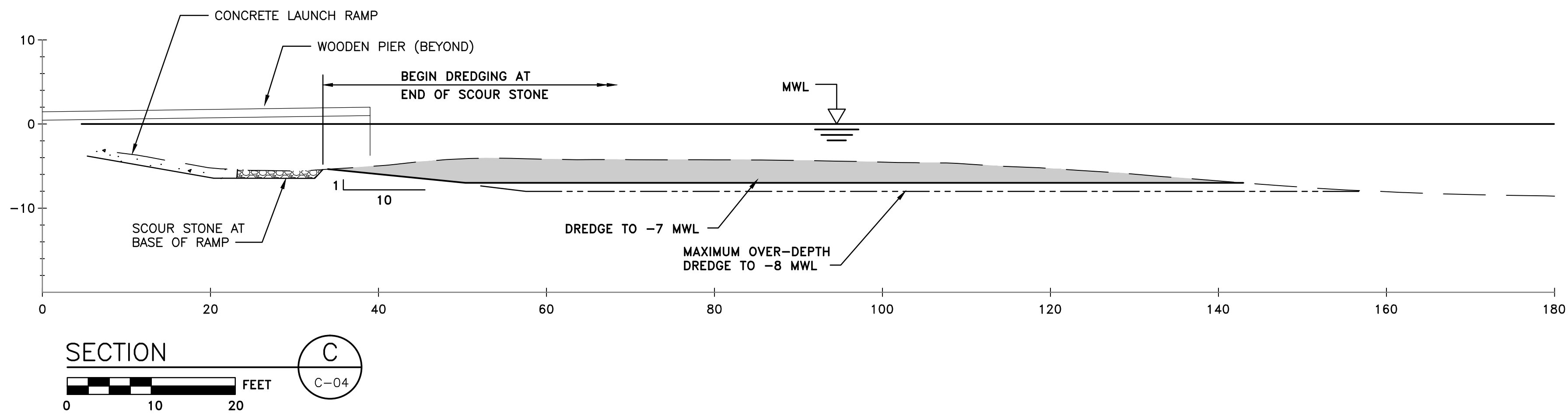
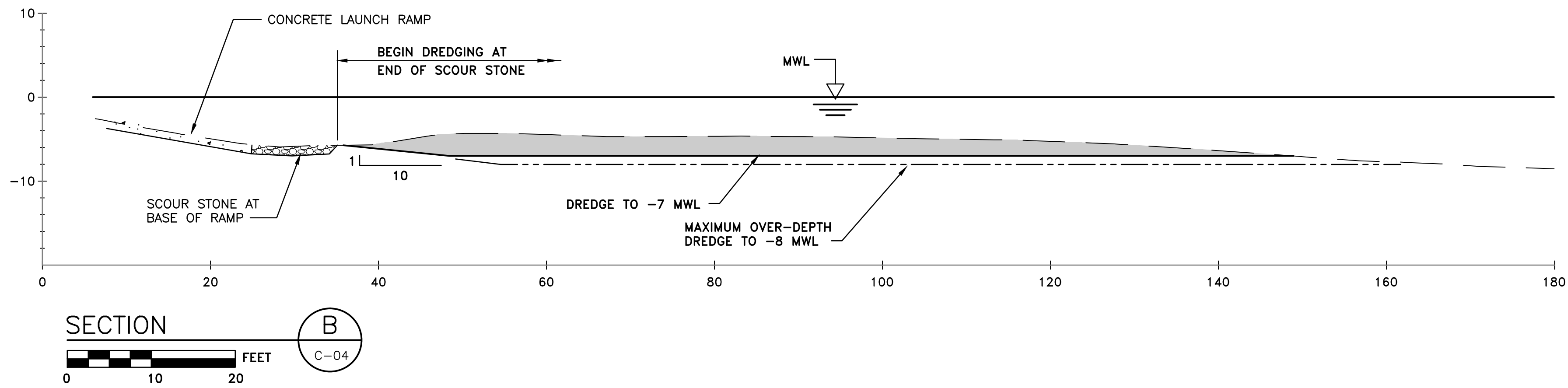
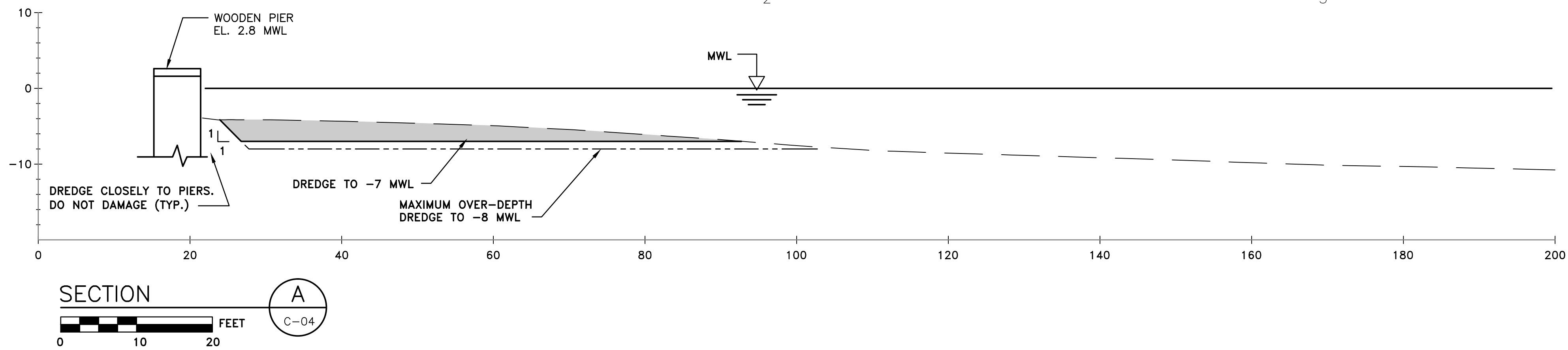


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ISSUE		

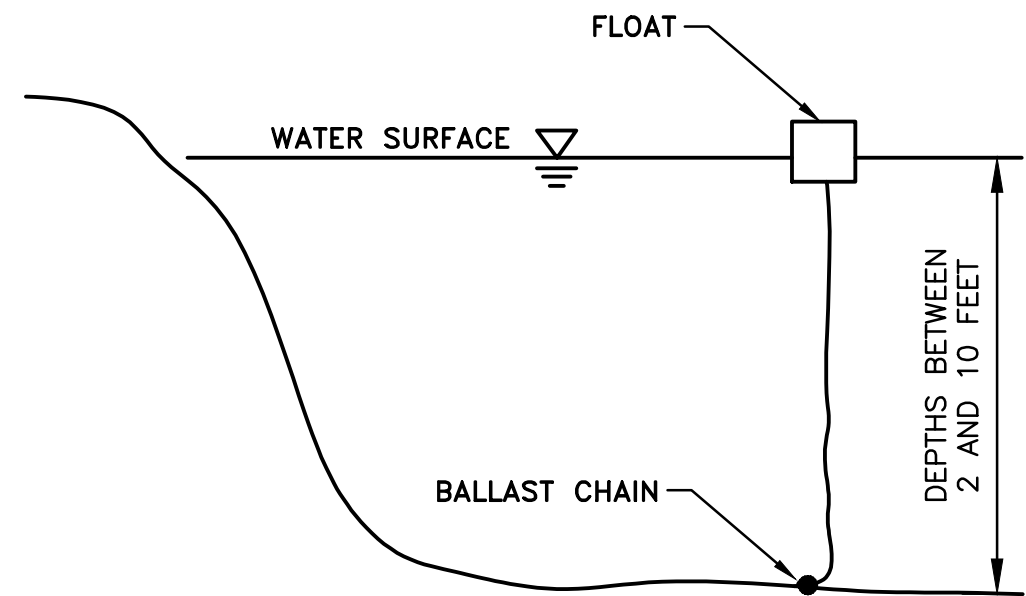
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DRAWN BY: MJC		
EDITED BY: MJC		
CHECKED BY: TAP		
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SHEET TITLE		
DREDGING AT MWR BOAT RAMP CG BASE ELIZABETH CITY ELIZABETH CITY NC MWR BOAT RAMP CIVIL DREDGE PLAN		
REVIEWED BY: J.W. NICKS	REVIEWED BY: G.P. LODGE	REVIEWED BY: G.S. PLACZEK
PROJECT ENG.	BRANCH CHIEF	TECH. DIRECTOR
P.R. CARROLL, CDR. APPROVING OFFICER		DATE
PROJECT NUMBER 3756419		DRAWING NUMBER 8257-D
DISCIPLINE/SHT NO C-03		SHEET 3 OF 4



LEGEND

- EXISTING TOP OF SEDIMENT
- PROPOSED BASE DREDGE ELEVATION
- - - MAXIMUM OVER-DEPTH DREDGE



TURBIDITY CURTAIN DETAIL
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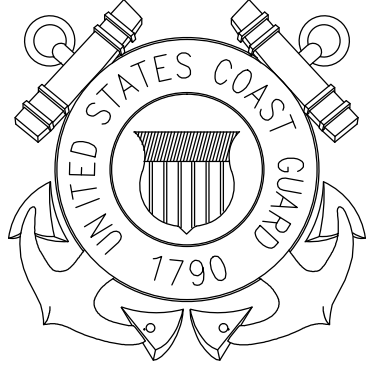
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ISSUE		
MARK	DATE	DESCRIPTION

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DESIGNED BY: SJR
DRAWN BY: MJC
EDITED BY: MJC
CHECKED BY: TAP

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SHEET TITLE

DREDGING AT MWR BOAT RAMP
CG BASE ELIZABETH CITY
ELIZABETH CITY NC
MWR BOAT RAMP
CIVIL
DREDGE CROSS SECTIONS

REVIEWED BY:	REVIEWED BY:	REVIEWED BY:
J.W. NICKS	G.P. LODGE	G.S. PLACZEK
PROJECT ENG.	BRANCH CHIEF	TECH. DIRECTOR

P.R. CARROLL, CDR.
APPROVING OFFICER

DATE

PROJECT NUMBER	DRAWING NUMBER
3756419	8257-D
DISCIPLINE/SHT NO	SHEET 4 OF 4
C-04	

ATTACHMENT (F)

REFERENCES CITED

References Cited

- National Oceanic and Atmospheric Administration, Office of Response and Restoration, Environmental Sensitivity Index Maps, 2011. Accessed online September 2014 at <http://response.restoration.noaa.gov/esi>
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**UNITED STATES DEPARTMENT OF COMMERCE**

National Oceanic and Atmospheric Administration

NATIONAL MARINE FISHERIES SERVICE

Southeast Regional Office

263 13th Avenue South

St. Petersburg, Florida 33701-5505

<http://sero.nmfs.noaa.gov>

F/SER31:KBD

Gregory O. Carpenter
Chief, Environmental Compliance
Civil Engineering Unit, Cleveland
United States Coast Guard
1240 East Ninth Street, Room 2179
Cleveland, Ohio 44199-2060

AUG 07 2015

Dear Mr. Carpenter:

This responds to your request for consultation with the National Marine Fisheries Service (NMFS), pursuant to Section 7 of the Endangered Species Act (ESA) for the following action.

Agency	NMFS Number	Project Type and Location
United States Coast Guard (USCG)	SER-2015-16144	Maintenance Dredging of the Morale, Welfare, and Recreation Boat Ramp at the USCG Base Elizabeth City

We received a letter from your project manager on January 15, 2015, requesting consultation for the project referenced above. The USCG is proposing to maintenance dredge the boat basin and moorings at the Morale, Welfare, and Recreation boat ramp at USCG Base Elizabeth City located at 1664 Weeksville Road in Pasquotank County, North Carolina. The project area is located approximately 10 nautical miles (nmi) from Albemarle Sound and approximately 30 nmi from the Atlantic Ocean at 36.2708°N, 76.1779°W (NAD 83). The USCG Civil Engineering Unit, Cleveland, determined that the project may affect, but is not likely to adversely affect, shortnose and Atlantic sturgeon. Our findings on the project's potential effects are based on the project descriptions in this response. Changes to the proposed actions for the project may negate our findings and may require reinitiating consultation.





Figure 1. Aerial view showing the site location in Pasquotank County, North Carolina (©2014 Google)

Project Description

The project area will be dredged to a depth of 8 feet below mean water level and the estimated volume of the dredge material is 755 cubic yards. Approximately 0.18 acres of the area will be temporarily blocked off and contained by an impermeable floating turbidity curtain with a weighted bottom edge that will be placed around the work area before and during dredging. A grab or clamshell dredge will be used, and the dredged material will be placed on a barge and transported to the East Carolina Regional Landfill. Work will be conducted during daylight hours only and will occur outside of the anadromous fish spawning moratorium (February 15 – June 30).

Shortnose Sturgeon

The area surrounding the project site provides habitat for anadromous fish. However, according to Fritz Rohde of the NMFS office in Beaufort, North Carolina, only a single record of a shortnose sturgeon has ever been reported near the area and that fish was found on the western side of Albemarle Sound, approximately 50 nmi from the project site. NMFS believes that any effects of the proposed project on shortnose sturgeon are extremely unlikely to occur because this species is not likely to be present in the project area. Therefore, effects are discountable and shortnose sturgeon will not be discussed further in this consultation.

Atlantic Sturgeon

Dredging Effects: We have identified a potential route of effects resulting from physical impacts of dredging to Atlantic sturgeon but concluded that Atlantic sturgeon are not likely to be adversely affected by the proposed action. Since the project area will be blocked off by a temporary barrier, sturgeon will not be able to enter the project site during dredging. Therefore, risk of injury from an encounter with the dredging equipment from sturgeon entering the project

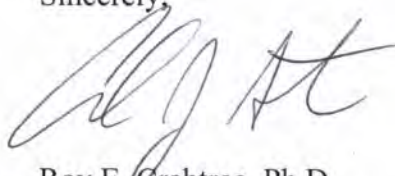
area is very limited. Furthermore, if a sturgeon was trapped within the temporary barrier, we believe sturgeon are highly mobile and could avoid the equipment as it will be a very slow dredging process using either a grab or a clamshell dredge. In addition, the barge holding the dredging equipment will be near stationary and when it is underway it will be moving very slowly. Since sturgeon are demersal, they are not likely to have adverse interactions with surface vessels. Therefore, we have determined that any potential impacts associated with equipment movement during in-water construction activities will be extremely unlikely and will therefore be discountable.

Foraging and Resting: We have identified a potential route of effects resulting from the temporary loss of access to the project site for foraging or resting because of dredging activities and concluded that Atlantic sturgeon are not likely to be adversely affected by the proposed action. We have determined all foraging and resting effects will be discountable as no foraging or resting habitat currently exists in the area due to the shallow depths and vessels' frequent use of the area.

Because all potential project effects to listed species were found to be discountable, we conclude that the proposed action is not likely to adversely affect listed species and critical habitat under NMFS's purview. This concludes the USCG's consultation responsibilities under the ESA for species under NMFS's purview. Consultation must be reinitiated if a take occurs or new information reveals effects of the action not previously considered, or the identified action is subsequently modified in a manner that causes an effect to the listed species or critical habitat in a manner or to an extent not previously considered, or if a new species is listed or critical habitat designated that may be affected by the identified action.

We have enclosed additional relevant information for your review. We look forward to further cooperation with you on other projects to ensure the conservation of our threatened and endangered marine species and designated critical habitat. If you have any questions on this consultation, please contact Kay Davy, Consultation Biologist, at (727) 415-9271, or by email at kay.davy@noaa.gov.

Sincerely,

A handwritten signature in black ink, appearing to read 'Roy E. Crabtree', written over a horizontal line.

For Roy E. Crabtree, Ph.D.
Regional Administrator

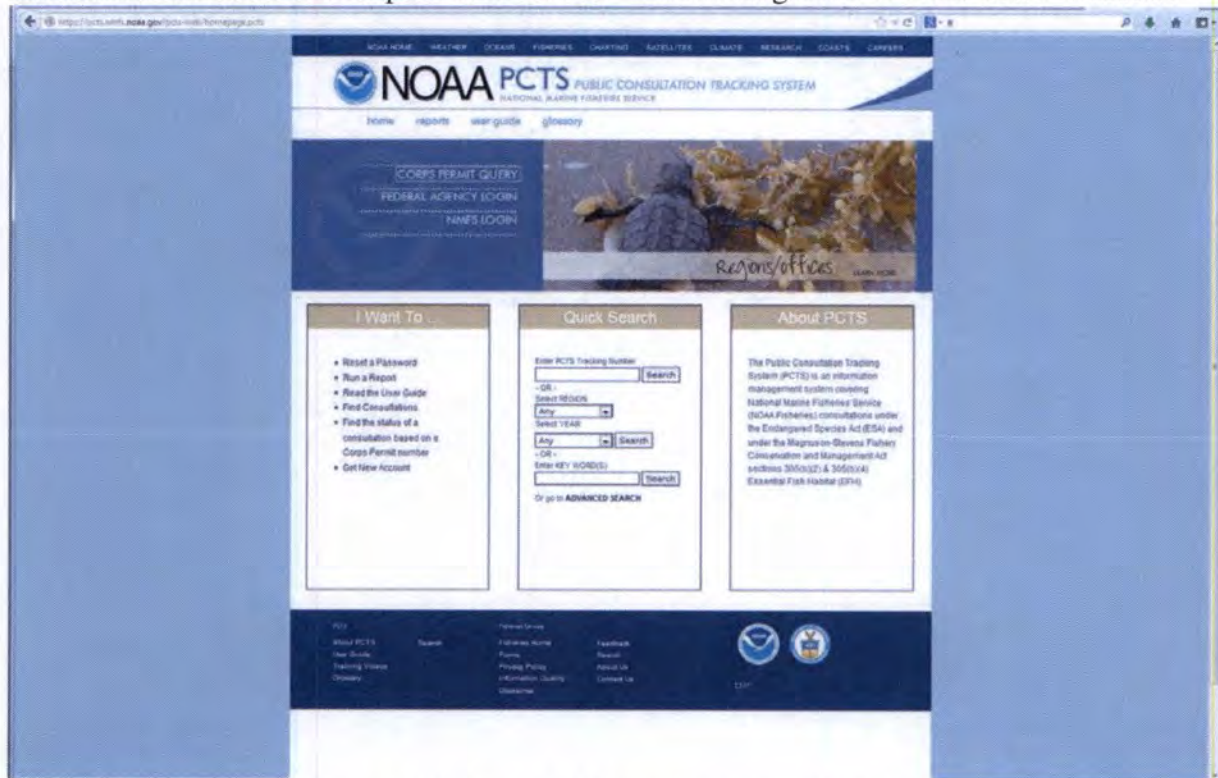
Enc.: *PCTS Access and Additional Considerations for ESA Section 7 Consultations*
(Revised March 10, 2015)

File: 1514-22.F.1

PCTS Access and Additional Considerations for ESA Section 7 Consultations (Revised 03-10-2015)

Public Consultation Tracking System (PCTS) Guidance: PCTS is a Web-based query system at <https://pcts.nmfs.noaa.gov/> that allows all federal agencies (e.g., U.S. Army Corps of Engineers - USACE), project managers, permit applicants, consultants, and the general public to find the current status of NMFS's Endangered Species Act (ESA) and Essential Fish Habitat (EFH) consultations which are being conducted (or have been completed) pursuant to ESA Section 7 and the Magnuson-Stevens Fishery Conservation and Management Act's (MSA) Sections 305(b)2 and 305(b)(4). Basic information including access to documents is available to all.

The PCTS Home Page is shown below. For USACE-permitted projects, the easiest and quickest way to look up a project's status, or review completed ESA/EFH consultations, is to click on either the "Corps Permit Query" link (top left); or, below it, click the "Find the status of a consultation based on the Corps Permit number" link in the golden "I Want To..." window.



Then, from the "Corps District Office" list pick the appropriate USACE district. In the "Corps Permit #" box, type in the 9-digit USACE permit number identifier, with no hyphens or letters. Simply enter the year and the permit number, joined together, using preceding zeros if necessary after the year to obtain the necessary 9-digit (no more, no less) number. For example, the USACE Jacksonville District's issued permit number SAJ-2013-0235 (LP-CMW) must be typed in as 201300235 for PCTS to run a proper search and provide complete and accurate results. For querying permit applications submitted for ESA/EFH consultation by other USACE districts, the procedure is the same. For example, an inquiry on Mobile District's permit MVN201301412 is entered as 201301412 after selecting the Mobile District from the "Corps District Office" list. PCTS questions should be directed to Kelly Shotts at Kelly.Shotts@noaa.gov or (727) 551-5603.

EFH Recommendations: In addition to its protected species/critical habitat consultation requirements with NMFS' Protected Resources Division pursuant to Section 7 of the ESA, prior to proceeding with the proposed action the action agency must also consult with NMFS' Habitat Conservation Division (HCD) pursuant to the MSA requirements for EFH consultation (16 U.S.C. 1855 (b)(2) and 50 CFR 600.905-.930, subpart K). The action agency should also ensure that the applicant understands the ESA and EFH processes; that ESA and EFH consultations are separate, distinct, and guided by different statutes, goals, and time lines for responding to the action agency; and that the action agency will (and the applicant may) receive separate consultation correspondence on NMFS letterhead from HCD regarding their concerns and/or finalizing EFH consultation.

Marine Mammal Protection Act (MMPA) Recommendations: The ESA Section 7 process does not authorize incidental takes of listed or non-listed marine mammals. If such takes may occur an incidental take authorization under MMPA Section 101 (a)(5) is necessary. Please contact NMFS' Permits, Conservation, and Education Division at (301) 713-2322 for more information regarding MMPA permitting procedures.



North Carolina Department of Administration

Pat McCrory, Governor

Bill Daughtridge, Jr., Secretary

June 2, 2015

Ms. Jennifer Jones
United States Coast Guard
c/o AECOM
701 Corporate Center Drive, Suite 475
Raleigh, North Carolina 27607

Re: SCH File # 15-E-0000-0602; EA; Proposed project is the Draft EA for the Dredging at the USCG Base Elizabeth City MWR Boat Ramp boat basin and moorings. Project will dredge the boat basin and moorings to a required depth below mean water level due to changing conditions at the site.

Dear Ms. Jones:

The above referenced environmental impact information has been submitted to the State Clearinghouse under the provisions of the National Environmental Policy Act. According to G.S. 113A-10, when a state agency is required to prepare an environmental document under the provisions of federal law, the environmental document meets the provisions of the State Environmental Policy Act. Attached to this letter for your consideration are the comments made by agencies in the course of this review.

If any further environmental review documents are prepared for this project, they should be forwarded to this office for intergovernmental review.

Should you have any questions, please do not hesitate to call.

Sincerely,

A handwritten signature in cursive script that reads "Crystal Best".

Crystal Best

State Environmental Review Clearinghouse

Attachments
cc: Region R

Mailing Address:
1301 Mail Service Center
Raleigh, NC 27699-1301

Telephone: (919)807-2425
Fax (919)733-9571
State Courier #51-01-00
e-mail state.clearinghouse@doa.nc.gov

Location Address:
116 West Jones Street
Raleigh, North Carolina



North Carolina Department of Environment and Natural Resources

Pat McCrory
Governor

Donald R. van der Vaart
Secretary

MEMORANDUM

TO: Crystal Best
State Clearinghouse

FROM: Lyn Hardison *LH*
Division of Environmental Assistance and Customer Service
Permit Assistance & Project Review Coordinator

RE: 15-0602
Environmental Assessment
Proposed project is the Draft EA for the Dredging at the USCG Base Elizabeth City MWR
Boat Ramp boat basin and moorings
Pasquotank County

Date: May 29, 2015

The Department of Environment and Natural Resources has reviewed the proposal for the referenced project. The comments are attached for the applicant's review.

The Department appreciates the cooperative efforts and open communication the applicant has with our agencies and we encourage these efforts to continue as they move forward with the project.

Thank you for the opportunity to respond.

Attachment

State of North Carolina
Department of Environment and Natural Resources
INTERGOVERNMENTAL REVIEW - PROJECT COMMENTS

Reviewing Office: Washington

Project Number 15-0602 Due Date: 5/27/2015

County Pasquotank

After review of this project it has been determined that the ENR permit(s) and/or approvals indicated may need to be obtained in order for this project to comply with North Carolina Law. Questions regarding these permits should be addressed to the Regional Office indicated on the reverse of the form. All applications, information and guidelines relative to these plans and permits are available from the same Regional Office.

	PERMITS	SPECIAL APPLICATION PROCEDURES or REQUIREMENTS	Normal Process Time (statutory time limit)
<input type="checkbox"/>	Permit to construct & operate wastewater treatment facilities, sewer system extensions & sewer systems not discharging into state surface waters.	Application 90 days before begin construction or award of construction contracts. On-site inspection. Post-application technical conference usual.	30 days (90 days)
<input type="checkbox"/>	NPDES - permit to discharge into surface water and/or permit to operate and construct wastewater facilities discharging into state surface waters.	Application 180 days before begin activity. On-site inspection. Pre-application conference usual. Additionally, obtain permit to construct wastewater treatment facility-granted after NPDES. Reply time, 30 days after receipt of plans or issue of NPDES permit-whichever is later.	90-120 days (N/A)
<input type="checkbox"/>	Water Use Permit	Pre-application technical conference usually necessary	30 days (N/A)
<input type="checkbox"/>	Well Construction Permit	Complete application must be received and permit issued prior to the installation of a well.	7 days (15 days)
<input type="checkbox"/>	Dredge and Fill Permit	Application copy must be served on each adjacent riparian property owner. On-site inspection. Pre-application conference usual. Filling may require Easement to Fill from N.C. Department of Administration and Federal Dredge and Fill Permit.	55 days (90 days)
<input type="checkbox"/>	Permit to construct & operate Air Pollution Abatement facilities and/or Emission Sources as per 15 A NCAC (2Q.0100 thru 2Q.0300)	Application must be submitted and permit received prior to construction and operation of the source. If a permit is required in an area without local zoning, then there are additional requirements and timelines (2Q.0113).	90 days
<input type="checkbox"/>	Permit to construct & operate Transportation Facility as per 15 A NCAC (2D.0800, 2Q.0601)	Application must be submitted at least 90 days prior to construction or modification of the source.	90 days
<input type="checkbox"/>	Any open burning associated with subject proposal must be in compliance with 15 A NCAC 2D.1900	N/A	60 days (90 days)
<input type="checkbox"/>	Demolition or renovations of structures containing asbestos material must be in compliance with 15 A NCAC 20.1110 (a) (1) which requires notification and removal prior to demolition. Contact Asbestos Control Group 919-707-5950.		
<input type="checkbox"/>	Complex Source Permit required under 15 A NCAC 2D.0800		
<input type="checkbox"/>	The Sedimentation Pollution Control Act of 1973 must be properly addressed for any land disturbing activity. An erosion & sedimentation control plan will be required if one or more acres to be disturbed. Plan filed with proper Regional Office (Land Quality Section) At least 30 days before beginning activity. A fee of \$65 for the first acre or any part of an acre. An express review option is available with additional fees.		20 days (30 days)
<input type="checkbox"/>	Sedimentation and erosion control must be addressed in accordance with NCDOT's approved program. Particular attention should be given to design and installation of appropriate perimeter sediment trapping devices as well as stable stormwater conveyances and outlets.		(30 days)
<input type="checkbox"/>	Mining Permit	On-site inspection usual. Surety bond filed with ENR Bond amount varies with type mine and number of acres of affected land. Any acre mined greater than one acre must be permitted. The appropriate bond must be received before the permit can be issued.	30 days (60 days)
<input type="checkbox"/>	North Carolina Burning permit	On-site inspection by N.C. Division Forest Resources if permit exceeds 4 days	1 day (N/A)
<input type="checkbox"/>	Special Ground Clearance Burning Permit - 22 counties in coastal N.C. with organic soils	On-site inspection by N.C. Division Forest Resources required "if more than five acres of ground clearing activities are involved. Inspections should be requested at least ten days before actual burn is planned."	1 day (N/A)
<input type="checkbox"/>	Oil Refining Facilities	N/A	90-120 days (N/A)
<input type="checkbox"/>	Dam Safety Permit	If permit required, application 60 days before begin construction. Applicant must hire N.C. qualified engineer to: prepare plans, inspect construction, certify construction is according to ENR approved plans. May also require permit under mosquito control program. And a 404 permit from Corps of Engineers. An inspection of site is necessary to verify Hazard Classification. A minimum fee of \$200.00 must accompany the application. An additional processing fee based on a percentage of the total project cost will be required upon completion.	30 days (60 days)

June 16, 2014

Project Number: <u>15-0602</u> Due Date: <u>5/27/2015</u>			Normal Process Time (statutory time limit)
PERMITS		SPECIAL APPLICATION PROCEDURES or REQUIREMENTS	
<input type="checkbox"/>	Permit to drill exploratory oil or gas well	File surety bond of \$5,000 with ENR running to State of NC conditional that any well opened by drill operator shall, upon abandonment, be plugged according to ENR rules and regulations.	10 days N/A
<input type="checkbox"/>	Geophysical Exploration Permit	Application filed with ENR at least 10 days prior to issue of permit. Application by letter. No standard application form.	10 days N/A
<input type="checkbox"/>	State Lakes Construction Permit	Application fee based on structure size is charged. Must include descriptions & drawings of structure & proof of ownership of riparian property.	15-20 days N/A
<input type="checkbox"/>	401 Water Quality Certification	N/A	60 days (130 days)
<input type="checkbox"/>	CAMA Permit for MAJOR development	\$250.00 fee must accompany application	55 days (150 days)
<input type="checkbox"/>	CAMA Permit for MINOR development	\$50.00 fee must accompany application	22 days (25 days)
<input type="checkbox"/>	Several geodetic monuments are located in or near the project area. If any monument needs to be moved or destroyed, please notify: N.C. Geodetic Survey, Box 27687 Raleigh, NC 27611		
<input type="checkbox"/>	Abandonment of any wells, if required must be in accordance with Title 15A, Subchapter 2C.0100.		
<input type="checkbox"/>	Notification of the proper regional office is requested if "orphan" underground storage tanks (USTs) are discovered during any excavation operation.		
<input type="checkbox"/>	Compliance with 15A NCAC 2H 1000 (Coastal Stormwater Rules) is required.		45 days (N/A)
<input type="checkbox"/>	Tar Pamlico or Neuse Riparian Buffer Rules required.		
<input type="checkbox"/>	Plans and specifications for the construction, expansion, or alteration of a public water system must be approved by the Division of Water Resources/Public Water Supply Section prior to the award of a contract or the initiation of construction as per 15A NCAC 18C .0300 et. seq. Plans and specifications should be submitted to 1634 Mail Service Center, Raleigh, North Carolina 27699-1634. All public water supply systems must comply with state and federal drinking water monitoring requirements. For more information, contact the Public Water Supply Section, (919) 707-9100.		30 days
<input type="checkbox"/>	If existing water lines will be relocated during the construction, plans for the water line relocation must be submitted to the Division of Water Resources/Public Water Supply Section at 1634 Mail Service Center, Raleigh, North Carolina 27699-1634. For more information, contact the Public Water Supply Section, (919) 707-9100.		30 days

Other comments (attach additional pages as necessary, being certain to cite comment authority)

Division	Initials	No comment	Comments	Date Review
DAQ		<input type="checkbox"/>		/ /
DWR-WQROS (Aquifer & Surface)	RLS	<input type="checkbox"/>	Recommend use of turbidity curtain.	/ / 5/12/15
DWR-PWS		<input type="checkbox"/>		/ /
DEMLR (LQ & SW)	PHM	<input checked="" type="checkbox"/>		5/11/15
DWM - UST		<input type="checkbox"/>		/ /

REGIONAL OFFICES

Questions regarding these permits should be addressed to the Regional Office marked below.

☐ **Asheville Regional Office**
2090 US Highway 70
Swannanoa, NC 28778
(828) 296-4500

☐ **Mooresville Regional Office**
610 East Center Avenue, Suite 301
Mooresville, NC 28115
(704) 663-1699

☐ **Wilmington Regional Office**
127 Cardinal Drive Extension
Wilmington, NC 28405
(910) 796-7215

☐ **Fayetteville Regional Office**
225 North Green Street, Suite 714
Fayetteville, NC 28301-5043
(910) 433-3300

☐ **Raleigh Regional Office**
3800 Barrett Drive, Suite 101
Raleigh, NC 27609
(919) 791-4200

☐ **Winston-Salem Regional Office**
585 Woughtown Street
Winston-Salem, NC 27107
(336) 771-5000

☒ **Washington Regional Office**
943 Washington Square Mall
Washington, NC 27889
(252) 946-6481

NORTH CAROLINA STATE CLEARINGHOUSE
DEPARTMENT OF ADMINISTRATION
INTERGOVERNMENTAL REVIEW

COUNTY: PASQUOTANK

H06: IMPOUNDMENTS AND NAVIGATION
DREDGING

STATE NUMBER: 15-E-0000-0602
DATE RECEIVED: 05/01/2015
AGENCY RESPONSE: 05/27/2015
REVIEW CLOSED: 06/01/2015

CLEARINGHOUSE COORD REGION R
ALBEMARLE REG PLANNING COMM
P.O. BOX 646
HERTFORD NC

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DENR - COASTAL MGT
DENR LEGISLATIVE AFFAIRS
DEPT OF AGRICULTURE
DEPT OF CULTURAL RESOURCES
DEPT OF TRANSPORTATION

PROJECT INFORMATION

APPLICANT: United States Coast Guard
TYPE: National Environmental Policy Act
Environmental Assessment

DESC: Proposed project is the Draft EA for the Dredging at the USCG Base Elizabeth City MWR Boat Ramp boat basin and moorings. Project will dredge the boat basin and moorings to a required depth below mean water level due to changing conditions at the site. - View documents at <http://www.uscg.mil/d5/PublicNotices.asp>

The attached project has been submitted to the N. C. State Clearinghouse for intergovernmental review. Please review and submit your response by the above indicated date to 1301 Mail Service Center, Raleigh NC 27699-1301.

If additional review time is needed, please contact this office at (919)807-2425.

AS A RESULT OF THIS REVIEW THE FOLLOWING IS SUBMITTED: ☒ NO COMMENT ☐ COMMENTS ATTACHED

SIGNED BY:

Cathy Dawson

DATE:

5.20.2015



NORTH CAROLINA STATE CLEARINGHOUSE
DEPARTMENT OF ADMINISTRATION
INTERGOVERNMENTAL REVIEW

Nazia Sarder

COUNTY: PASQUOTANK

H06: IMPOUNDMENTS AND NAVIGATION
DREDGING

STATE NUMBER: 15-E-0000-0602
DATE RECEIVED: 05/01/2015
AGENCY RESPONSE: 05/27/2015
REVIEW CLOSED: 06/01/2015

MS CARRIE ATKINSON
CLEARINGHOUSE COORDINATOR
DEPT OF TRANSPORTATION
STATEWIDE PLANNING - MSC #1554
RALEIGH NC

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DEPT OF AGRICULTURE
DEPT OF CULTURAL RESOURCES
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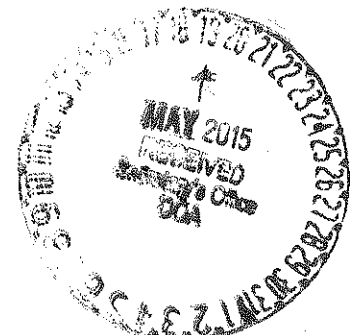
If additional review time is needed, please contact this office at (919)807-2425.

AS A RESULT OF THIS REVIEW THE FOLLOWING IS SUBMITTED: ☒ NO COMMENT ☐ COMMENTS ATTACHED

SIGNED BY:

[Signature]

DATE: *May 13, 15*



NORTH CAROLINA STATE CLEARINGHOUSE
DEPARTMENT OF ADMINISTRATION
INTERGOVERNMENTAL REVIEW

COUNTY: PASQUOTANK

H06: IMPOUNDMENTS AND NAVIGATION
DREDGING

STATE NUMBER: 15-E-0000-0602
DATE RECEIVED: 05/01/2015
AGENCY RESPONSE: 05/27/2015
REVIEW CLOSED: 06/01/2015

MS CAROLYN PENNY
CLEARINGHOUSE COORDINATOR
CC&PS - DIV OF EMERGENCY MANAGEMENT
FLOODPLAIN MANAGEMENT PROGRAM
MSC # 4719
RALEIGH NC

MAY 7 2015

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DENR - COASTAL MGT
DENR LEGISLATIVE AFFAIRS
DEPT OF AGRICULTURE
DEPT OF CULTURAL RESOURCES
DEPT OF TRANSPORTATION

PROJECT INFORMATION

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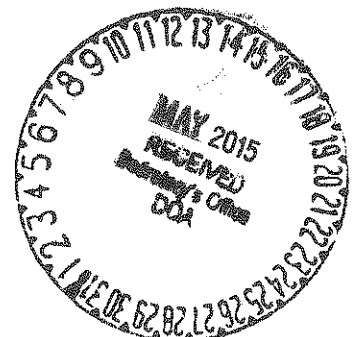
If additional review time is needed, please contact this office at (919)807-2425.

AS A RESULT OF THIS REVIEW THE FOLLOWING IS SUBMITTED: ☒ NO COMMENT ☐ COMMENTS ATTACHED

SIGNED BY:

John D. Burkholder

DATE: 11 May 2015

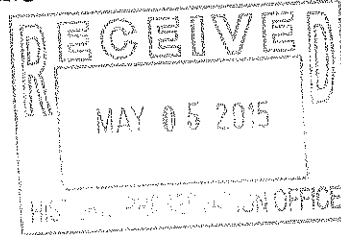


NORTH CAROLINA STATE CLEARINGHOUSE
DEPARTMENT OF ADMINISTRATION
INTERGOVERNMENTAL REVIEW

COUNTY: PASQUOTANK

H06: IMPOUNDMENTS AND NAVIGATION
DREDGING

STATE NUMBER: 15-E-0000-0602
DATE RECEIVED: 05/01/2015
AGENCY RESPONSE: 05/27/2015
REVIEW CLOSED: 06/01/2015



MS RENEE GLEDHILL-EARLEY
CLEARINGHOUSE COORDINATOR
DEPT OF CULTURAL RESOURCES
STATE HISTORIC PRESERVATION OFFICE
MSC 4617 - ARCHIVES BUILDING
RALEIGH NC

EX 14-1676

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DENR - COASTAL MGT
DENR LEGISLATIVE AFFAIRS
DEPT OF AGRICULTURE
DEPT OF CULTURAL RESOURCES
DEPT OF TRANSPORTATION

Previously Cleared

PROJECT INFORMATION

APPLICANT: United States Coast Guard
TYPE: National Environmental Policy Act
Environmental Assessment

DESC: Proposed project is the Draft EA for the Dredging at the USCG Base Elizabeth City MWR Boat Ramp boat basin and moorings. Project will dredge the boat basin and moorings to a required depth below mean water level due to changing conditions at the site. - View documents at <http://www.uscg.mil/d5/PublicNotices.asp>

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AS A RESULT OF THIS REVIEW THE FOLLOWING IS SUBMITTED: ☒ NO COMMENT ☐ COMMENTS ATTACHED

SIGNED BY:

Renee Gledhill-Earley

DATE:

5.6.15



Appendix E.

State Historic Preservation Office Consultation



AECOM
701 Corporate Center Drive, Suite 475
Raleigh, NC 27607

919.854.6200 tel
919.854.6259 fax

July 18, 2014

Ms. Renee Gledhill-Earley
State Historic Preservation Office
4617 Mail Service Center
Raleigh, NC 27699-4617

SUBJECT: U.S. Coast Guard Base Elizabeth City – Dredging at MWR Boat Ramp, Elizabeth City, North Carolina

Dear Ms. Gledhill-Earley:

In compliance with Section 106 of the National Historic Preservation Act (NHPA), the U.S. Coast Guard (USCG) Civil Engineering Unit Cleveland is initiating consultation with your office concerning the proposed action to complete dredging of the boat ramp and moorings at the Morale, Welfare and Recreate (MWR) Boat Ramp at the USCG Base Elizabeth City. The USCG Base Elizabeth City is located approximately 3.5 miles southeast of Elizabeth City, along Highway 34, on the south shore of the Pasquotank River in Pasquotank County, North Carolina (**Attachment A**). The geographic location of the MWR Boat Ramp is latitude 36° 16' 14.72" north and longitude 76° 10' 40.31" west. The USCG has approved this consultation packet pursuant to 36 CFR 800.2(a)(3).

The base is the largest USCG Air Station in the nation, and contains a number of commands - Air Station Elizabeth City, Aircraft Repair and Supply Center, Aviation Technical Training Center, Support Center Elizabeth City, and Small Boat Station Elizabeth City. The base was established in 1940 and consists of over 100 buildings which have been constructed over the lifetime of the base. The base occupies approximately 950 acres and most of the base's buildings support the air command. The base can be accessed either from the main gate off Weeksville Road on the north end of the base, or from a second entrance off Consolidated Road on the south end of the base. The MWR Boat Ramp can be approached by water from the northeast or by land via Moukawsher Drive along the shoreline (**Attachment B**).

The MWR Boat Ramp is located on the north end of the base's shoreline, in a portion of the river tidal flats. The MWR Boat Ramp consists of a two-lane concrete ramp with a concrete pavement approach, and a launch dock centered in the boat ramp. Rubblemound stone breakwaters project approximately 100 to 125 feet from the shoreline on both sides of the launch ramp to form a protected harbor area for launching and retrieving boats. Timber docks line both sides of this protected harbor area. Mooring piles are in place along the west timber dock. Navigation aids are located near the entrance to the protected harbor area. These consist of a red dayboard with a solar-powered red beacon, and a green dayboard (**Attachment C**).

The MWR Boat Ramp is a two-lane concrete surface launch ramp utilized by base military personnel for launching privately-owned recreational boats, and for launching USCG vessels stationed at the Building 43 moorings. The purpose of this project is to dredge the boat basin and moorings to a required depth below Mean Water Low (MWL) datum at the MWR Boat Ramp in order for the unit's small boats to transit and be able to execute their operational missions without the encumbrances of shoaling in the boat basin that they currently are experiencing due to specific wind directions at low tide. Maintenance dredging has not previously been conducted at the MWR Boat Ramp. The USCG anticipates conducting the proposed

maintenance dredging activities under USACE Nationwide Permit #35. For the MWR Boat Ramp, the USCG Area of Potential Effects (APE) includes dredging to a maximum depth of 8 feet below MWL (includes one foot over dredge). The dredge volume for the MWR Boat Ramp is 755 cubic yards. The 35% civil site plan entitled, "Dredging at MWR Boat Ramp: CG Base Elizabeth City" illustrates the location and specifications of this proposed dredging project (**Attachment D**).

The USCG has researched multiple options for disposal of dredge material for this project. The East Carolina Regional Landfill, located at 1922 Republican Road in Aulander, Bertie County, North Carolina, is approximately 63 miles from Base Elizabeth City. This landfill is a licensed and approved landfill that will accept dredged material. Therefore, the APE includes the East Carolina Regional Landfill since it is the likely recipient of dredge spoils for this proposed project.

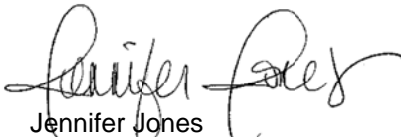
The USCG completed background research and records review using the State Historic Preservation Office (SHPO) Web GIS application, the online files of the North Carolina Office of State Archaeology, and the National Register of Historic Places. Based on the USCG's review, there have been no prior archaeological investigations and no historic architectural surveys completed for the APE. There are no National Register-listed or -eligible, or potentially eligible (according to the SHPO Study List) terrestrial or underwater archaeological sites or historic architectural resources located within or immediately adjacent to the APE. Three historic architectural resources have been identified within one mile of the APE, including one site that has been Determined Eligible and two potentially eligible sites on the SHPO Study List. No shipwrecks have been recorded in the MWR boat ramp and moorings APE and no historic properties have been recorded at the East Carolina Regional Landfill.

The Thrun Hall Barracks is a site that has been determined National Register-eligible in 2010 and is located inland from the MWR Boat Ramp, approximately three-quarters of a mile to the southeast of the APE. The Thrun Hall Barracks were subsequently photo documented and demolished upon the SHPO's approval. The John Hollowell House (PK0972) and the C.W. Hollowell House (PK0474) are on the Study List and are located inland from the MWR Boat Ramp, approximately one mile to the south-southwest of the APE (**Attachment E**).

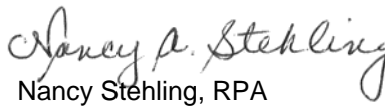
The proposed dredging activities will not create any subsurface disturbance that could affect terrestrial archaeological resources, if present. Dredging will not be staged on the shore, and all dredged sediment will be loaded onto a barge. No previously identified shipwrecks or other underwater archaeological resources have been recorded for the MWR boat ramp and moorings APE or for the immediate vicinity of the APE. Although maintenance dredging has not been conducted for the boat ramp and moorings, the actions required for the original construction of the two-lane concrete ramp, launch dock, wooden piers, and associated moorings have undoubtedly disturbed the sediments to a considerable depth. Due to the extent of prior subsurface disturbance in the MWR boat ramp and moorings APE, it is unlikely that intact, significant underwater archaeological resources would be present and retain integrity. Therefore, the USCG has determined "No Historic Properties Affected." We would appreciate your concurrence with this determination.

If you have any questions, please contact Jennifer Jones (AECOM) at (919) 239-7150 or Leilani Woods (USCG) at (252) 335-6847.

Yours sincerely,



Jennifer Jones
Environmental Task Manager



Nancy Stehling, RPA
Senior Archaeologist

Attachments: (A) Dredge Site - Location Map
(B) Dredge Site - Aerial Photograph
(C) Dredge Site - Ground-Level Photographs
(D) Dredge Site – 35% Plan
(E) Historic Preservation Office – Web GIS Maps
(F) References Cited

CC: Ms. Leilani Woods, Environmental Engineer, USCG Civil Engineering Unit Cleveland
Mr. Greg Carpenter, Environmental Project Manager, USCG Civil Engineering Unit Cleveland

ATTACHMENT (A)

DREDGE SITE - LOCATION MAP



Map Location



Site Location Map

MWR Boat Ramp
USCG Base Elizabeth City, North Carolina
Elizabeth City NC Topographic Quadrangle (1983)

June 2014

0 1,000 2,000
Feet

60321114

Figure 1

AECOM

AECOM Environment
701 Corporate Center Drive, Suite 475
Raleigh, NC 27607
Phone: (919) 854-6200
Fax: (919) 854-6259
Web: <http://www.aecom.com>

ATTACHMENT (B)

DREDGE SITE – AERIAL PHOTOGRAPH



Scale: Not To Scale

(Source: Google Earth© 2014)

Dredge Site - Aerial Photograph

US Coast Guard
Base Elizabeth City – MWR Boat Ramp
Elizabeth City, NC

AECOM

Attachment B

www.aecom.com

ATTACHMENT (C)

DREDGE SITE – GROUND-LEVEL PHOTOGRAPHS

Facility Name: United States Coast Guard	Site Location: MWR Boat Ramp – USCG Base Elizabeth City, NC	Project No. 60321114
--	---	--------------------------------

Photo No. 1	Date: 05/7/14	
Direction Photo Taken: North		
Description: View from the southern side of the shore (concrete pavement approach) of the concrete ramp, docks, navigation aids, and light pedestals.		

Photo No. 2	Date: 05/7/14	
Direction Photo Taken: Northwest		
Description: View from the concrete pavement approach along the southern shoreline.		

Facility Name: United States Coast Guard	Site Location: MWR Boat Ramp – USCG Base Elizabeth City, NC	Project No. 60321114
--	---	--------------------------------

Photo No. 3	Date: 05/7/14	
Direction Photo Taken: South		
Description: View looking south of the concrete ramp, docks, light pedestals, and mooring piles.		

Photo No. 4	Date: 05/7/14
Direction Photo Taken: North	
Description: View at the end of the concrete ramp of the docks, light pedestals, and mooring piles.	



Facility Name: United States Coast Guard	Site Location: MWR Boat Ramp – USCG Base Elizabeth City, NC	Project No. 60321114
--	---	--------------------------------

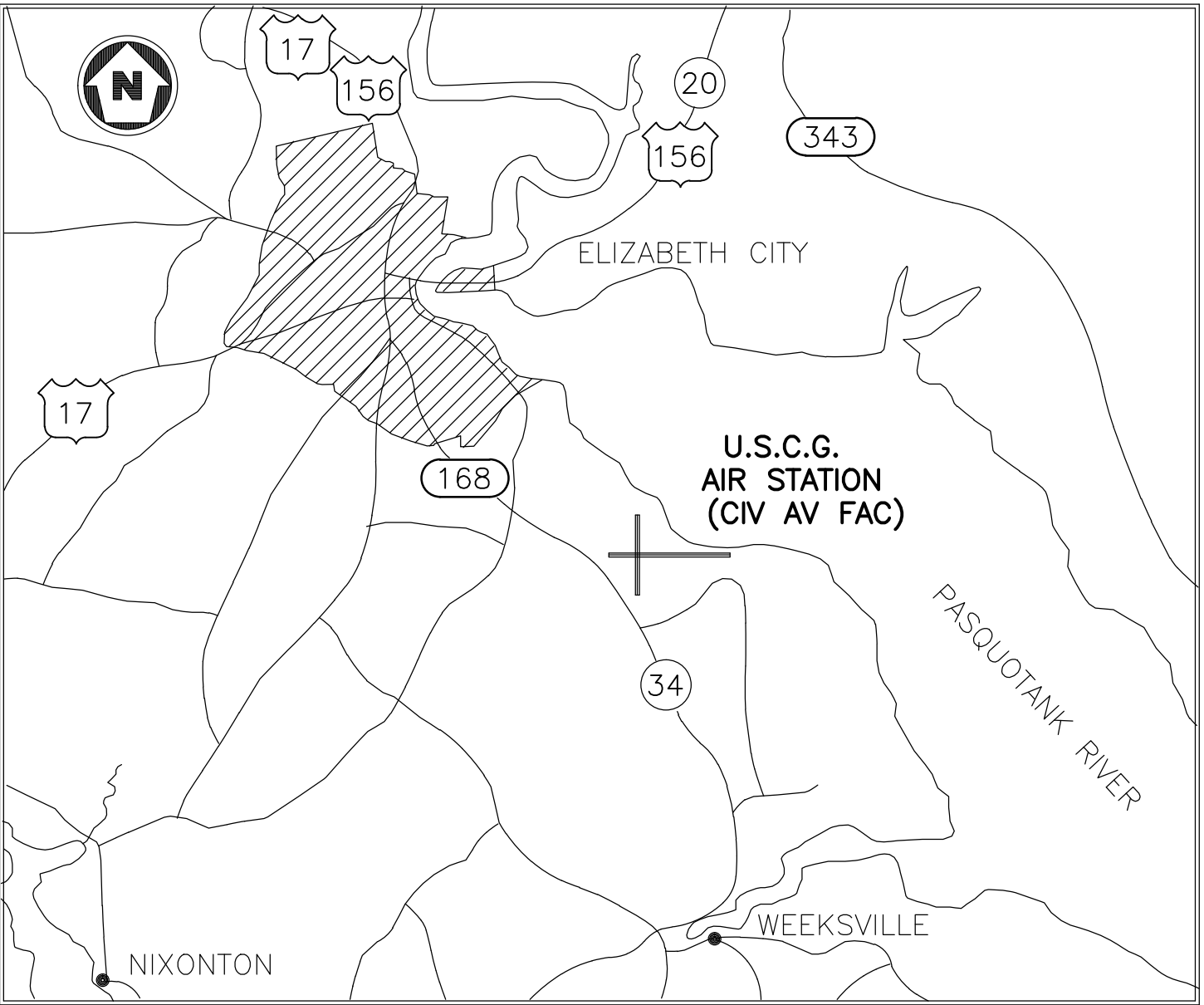
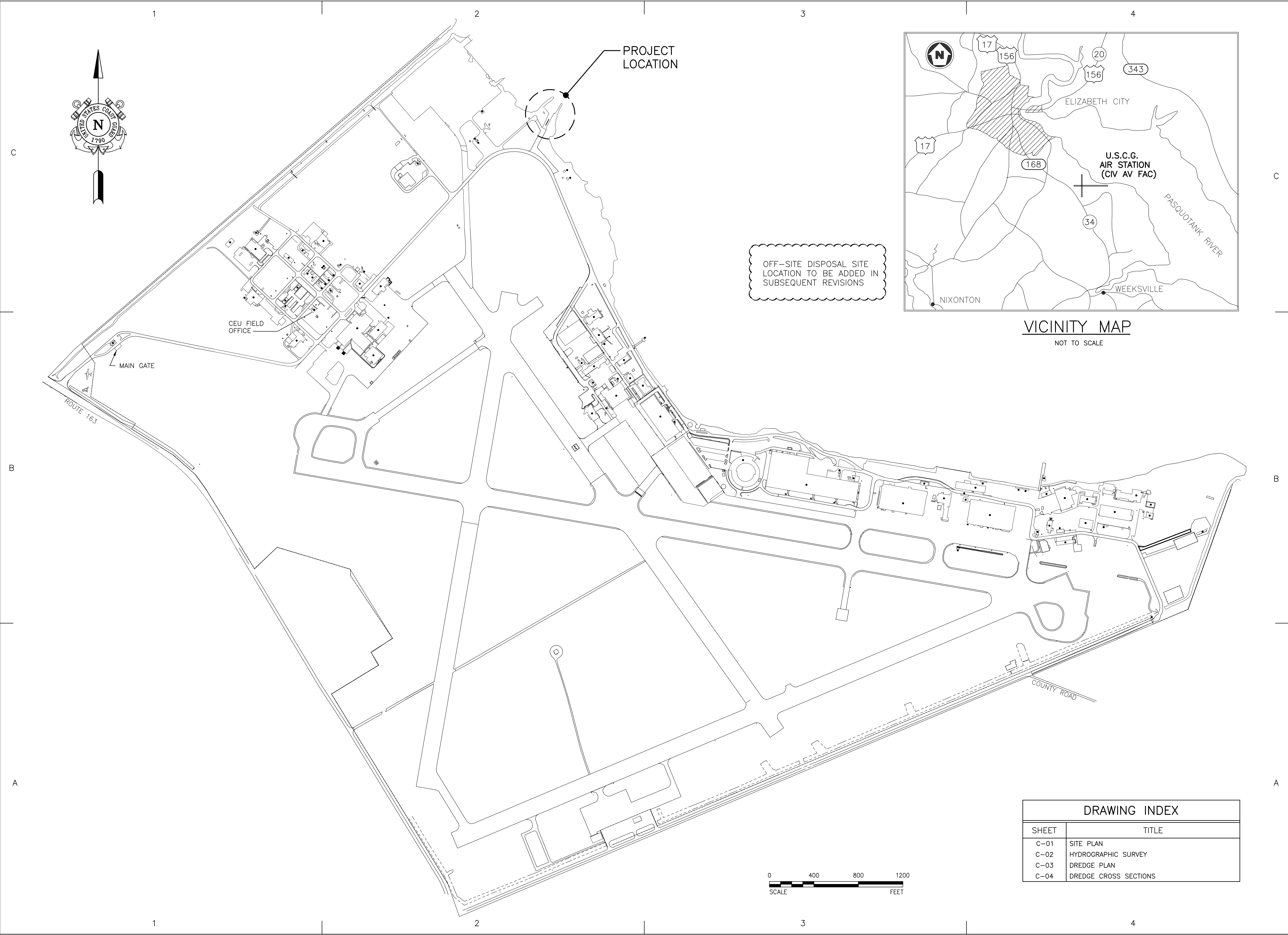
Photo No. 5	Date: 05/7/14
Direction Photo Taken: South-Southeast	
Description: View of the man-made beach located southeast of the MWR Boat Ramp.	



Photo No. 6	Date: 05/7/14	
Direction Photo Taken: North		
Description: View of the navigation aids at entry to the protected harbor area. Note two timbers low to water surface in the entry-way.		

ATTACHMENT (D)

DREDGE SITE – 35% PLAN



VICINITY MAP

NOT TO SCALE

DRAWING INDEX

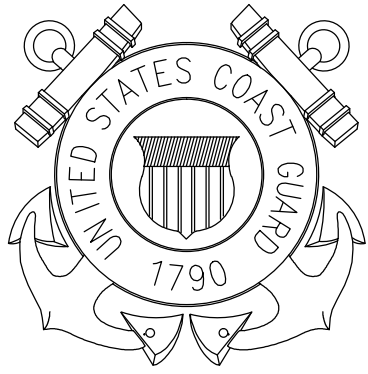
SHEET	TITLE
C-01	SITE PLAN
C-02	HYDROGRAPHIC SURVEY
C-03	DREDGE PLAN
C-04	DREDGE CROSS SECTIONS

USCG CEU
CLEVELAND, OHIO
(216) 902-6200

CONSULTANTS

AECOM
8320 Guilford Road, Suite L
Columbia, MD 21046

U. S. COAST GUARD
CIVIL ENGINEERING UNIT
CLEVELAND



USCG. CEU CLEVELAND
1240 EAST 9TH STREET
CLEVELAND, OH 44199-2060

ISSUE		
MARK	DATE	DESCRIPTION

A/E PROJECT NO: 60321114
CAD FILE NAME: C8257C01.DWG
DESIGNED BY: SJR
DRAWN BY: MJC
EDITED BY: MJC
CHECKED BY: TAP

SCALE: AS SHOWN PLOT SCALE: 1:1

SHEET TITLE

DREDGING AT MWR BOAT RAMP
CG BASE ELIZABETH CITY
ELIZABETH CITY NC
MWR BOAT RAMP
CIVIL
SITE PLAN

REVIEWED BY: J.W. NICKS PROJECT ENG.	REVIEWED BY: G.P. LODGE BRANCH CHIEF	REVIEWED BY: G.S. PLACZEK TECH. DIRECTOR
--	--	--

P.R. CARROLL, CDR. APPROVING OFFICER	DATE
---	------

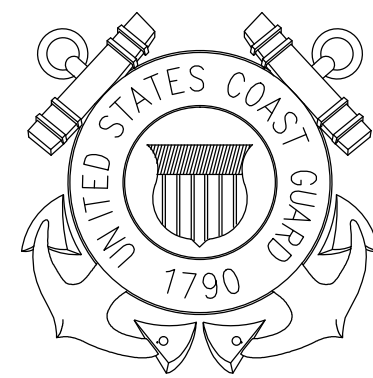
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DISCIPLINE/SHT NO C-01	SHEET 1 OF 4

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Columbia, MD 21046

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CIVIL ENGINEERING UNIT
CLEVELAND



USCG. CEU CLEVELAND
1240 EAST 9TH STREET
CLEVELAND, OH 44199-2060

ISSUE

MARK	DATE	DESCRIPTION

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DRAWN BY: MJC

EDITED BY:	MJC
DATE:	11/11/2011

CHECKED BY:

SCALE: AS SHOWN PLOT SCALE: 1:1

SHEET TITLE

DREDGING AT MWR BOAT RAMP
CG BASE ELIZABETH CITY
ELIZABETH CITY NC
MWR BOAT RAMP
CIVIL
DREDGE PLAN

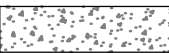

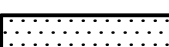
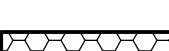

REVIEWED BY:	REVIEWED BY:	REVIEWED BY:
J.W. NICKS	G.P. LODGE	G.S. PLACZEK
PROJECT ENG.	BRANCH CHIEF	TECH. DIRECTOR

P.R. CARROLL, CDR.	
APPROVING OFFICER	DATE

PROJECT NUMBER	DRAWING NUMBER
3756410	8257 D

DISCIPLINE/SHT NO	
C-03	SHEET 3 OF 4

LEGEND:

- | | |
|---|--|
| — -5.0 — | EXISTING CONTOUR (5' INTERVAL) |
| --- -1.0 --- | EXISTING CONTOUR (1' INTERVAL) |
| ◆ | DREDGE COORDINATE POINT & ID |
| 11.3 | DEPTH MEASUREMENT (SOUNDING)
REFERENCED TO MEAN WATER LEVEL (MWL) |
| -10 | CONTOUR ELEVATION REFERENCED TO MWL |
| ⊕2.5 | SPOT ELEVATION (ON PIER OR STRUCTURE) |
|  | EXISTING CONCRETE MATERIAL |
|  | EXISTING RIPRAP |
|  | PROPOSED DREDGING TO TARGET
DEPTH -7 MWL |
|  | PROPOSED OVER-DEPTH DREDGE
TO -8 MWL |
|  | PROPOSED DREDGE SIDE SLOPES
(SLOPES VARY - SEE PLAN) |

NOTES:

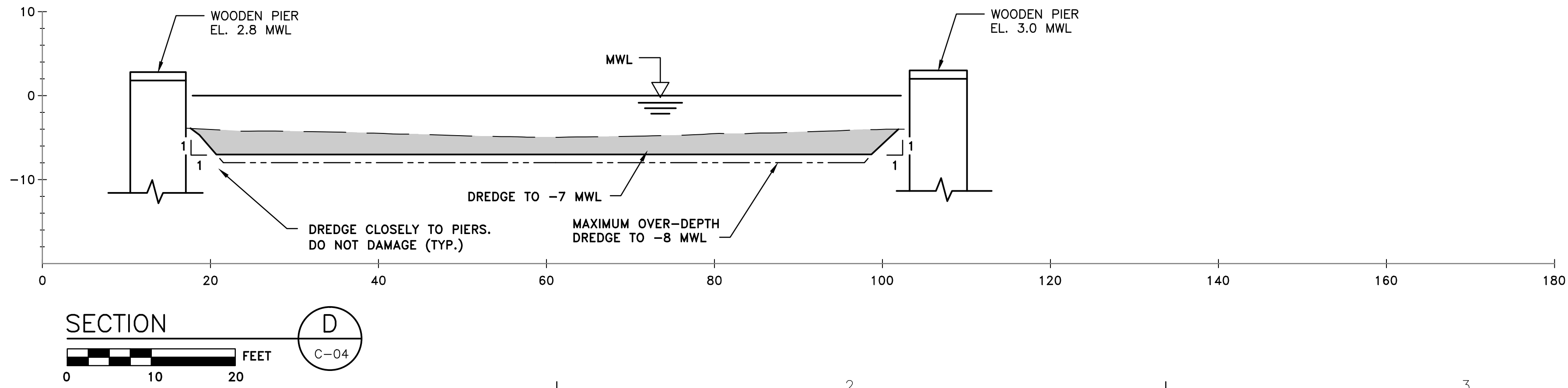
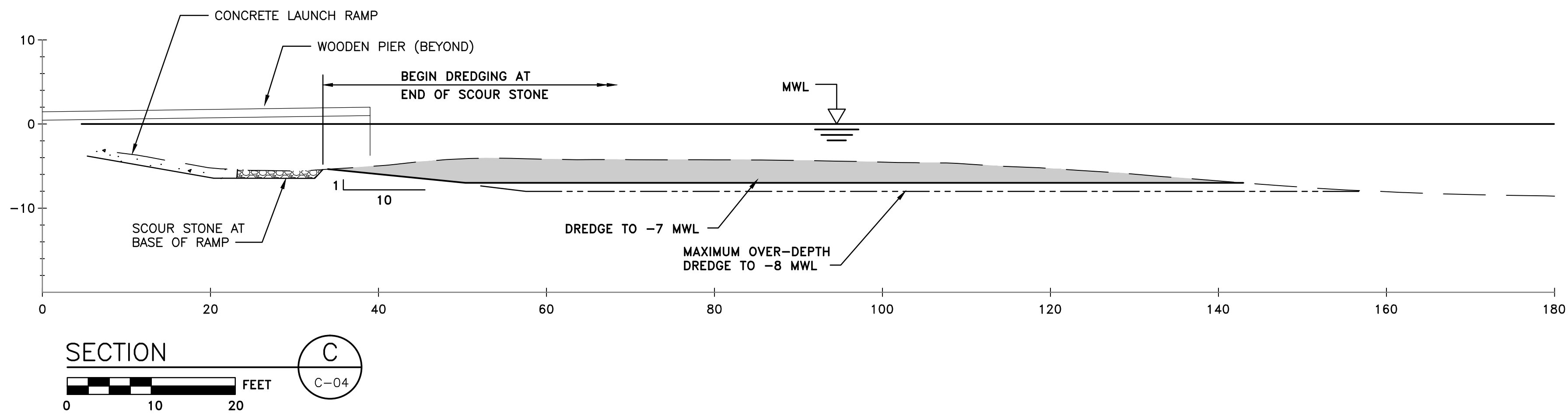
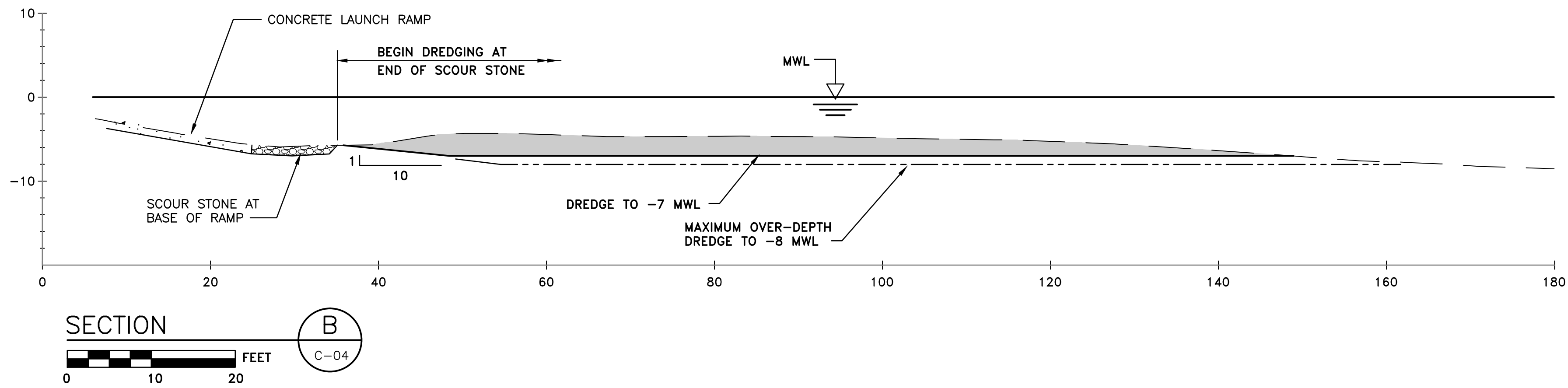
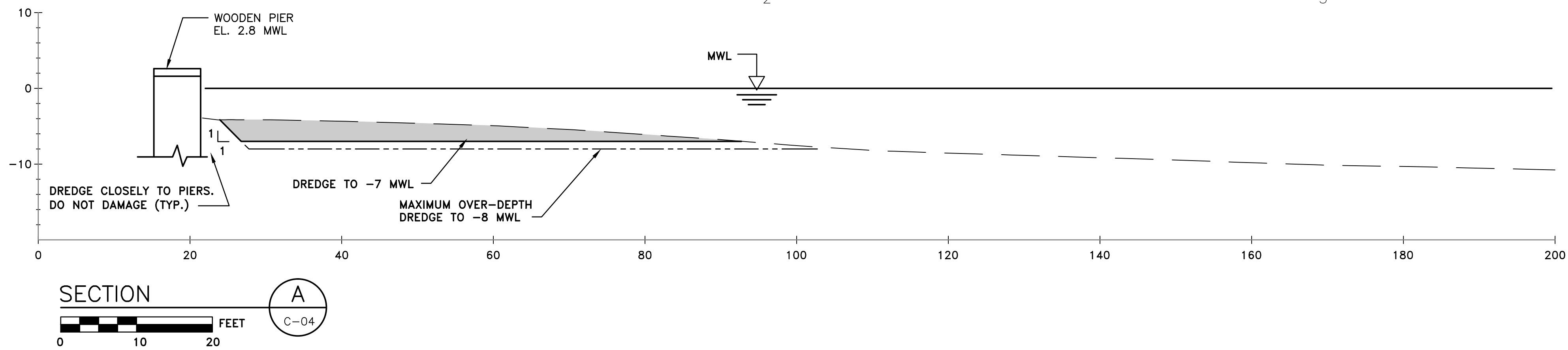
1. 0.0 MWL FOR PASQUOTANK RIVER = $-0.44'$ NAVD88
2. THE TARGET DREDGE DEPTH FOR THIS PROJECT IS $-7'$ FT MWL. OVER-DEPTH DREDGE IN THESE AREAS SHALL NOT EXCEED $-8'$ FT MWL.
3. CONTRACTOR SHALL USE METHODS AND EQUIPMENT THAT PROTECT THE PIER STRUCTURES FROM DAMAGE. CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE.
4. CONTRACTOR SHALL DREDGE AT A 10H:1V SLOPE AT THE LAUNCH RAMP STARTING AT THE END OF THE RIPRAP SCOUR PROTECTION.
5. MORATORIUM: TO COMPLY WITH PERMITS, NO DREDGING SHALL BE CONDUCTED BETWEEN OCTOBER 1 AND FEBRUARY 15.

DREDGE COORDINATE TABLE

DREDGE ID	NORTHING	EASTING
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D2	929337.45	2831913.52

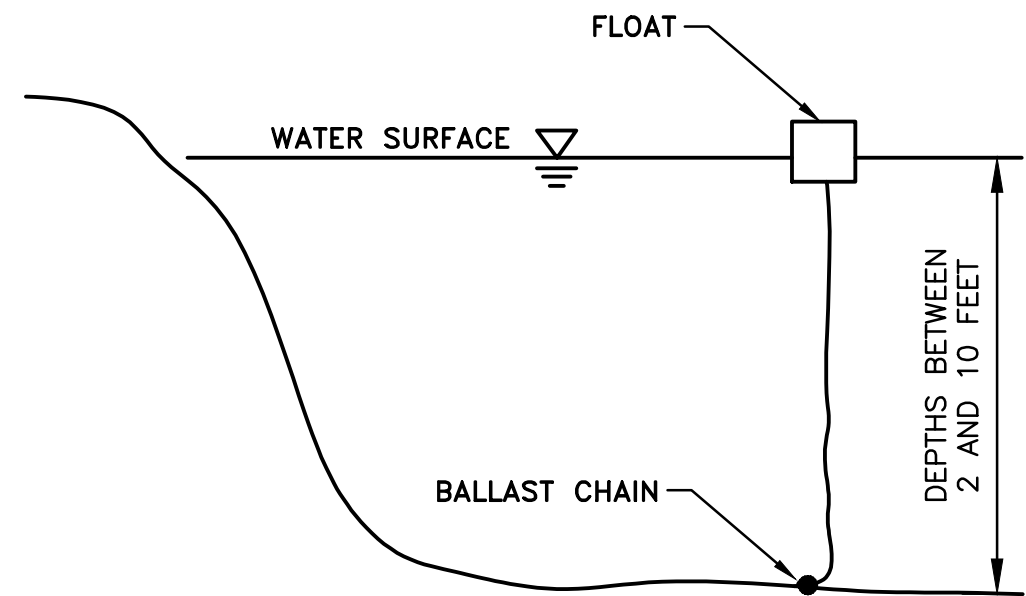
- COORDINATES ARE IN NORTH CAROLINA STATE PLANE GRID COORDINATE SYSTEM, FEET (NAD83).





LEGEND

- EXISTING TOP OF SEDIMENT
- PROPOSED BASE DREDGE ELEVATION
- MAXIMUM OVER-DEPTH DREDGE



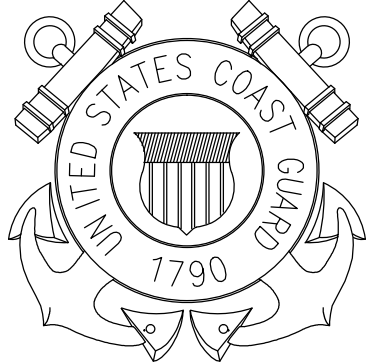
USCG CEU
CLEVELAND, OHIO
(216) 902-6200

CONSULTANTS

AECOM

8320 Guilford Road, Suite L
Columbia, MD 21046

U. S. COAST GUARD
CIVIL ENGINEERING UNIT
CLEVELAND



USCG. CEU CLEVELAND
1240 EAST 9TH STREET
CLEVELAND, OH 44199-2060

ISSUE		
MARK	DATE	DESCRIPTION

A/E PROJECT NO: 60321114
CAD FILE NAME: C8257C04.DWG
DESIGNED BY: SJR
DRAWN BY: MJC
EDITED BY: MJC
CHECKED BY: TAP

SCALE: AS SHOWN PLOT SCALE: 1:1

SHEET TITLE

DREDGING AT MWR BOAT RAMP
CG BASE ELIZABETH CITY
ELIZABETH CITY NC
MWR BOAT RAMP
CIVIL
DREDGE CROSS SECTIONS

REVIEWED BY:	REVIEWED BY:	REVIEWED BY:
J.W. NICKS	G.P. LODGE	G.S. PLACZEK
PROJECT ENG.	BRANCH CHIEF	TECH. DIRECTOR

P.R. CARROLL, CDR.
APPROVING OFFICER

DATE

PROJECT NUMBER	DRAWING NUMBER
3756419	8257-D
DISCIPLINE/SHT NO	SHEET 4 OF 4
C-04	

ATTACHMENT (E)

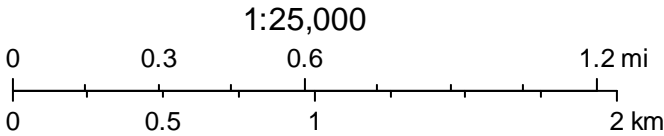
HISTORIC PRESERVATION OFFICE – WEB GIS MAPS

Base Elizabeth City_MWR Boat Ramp



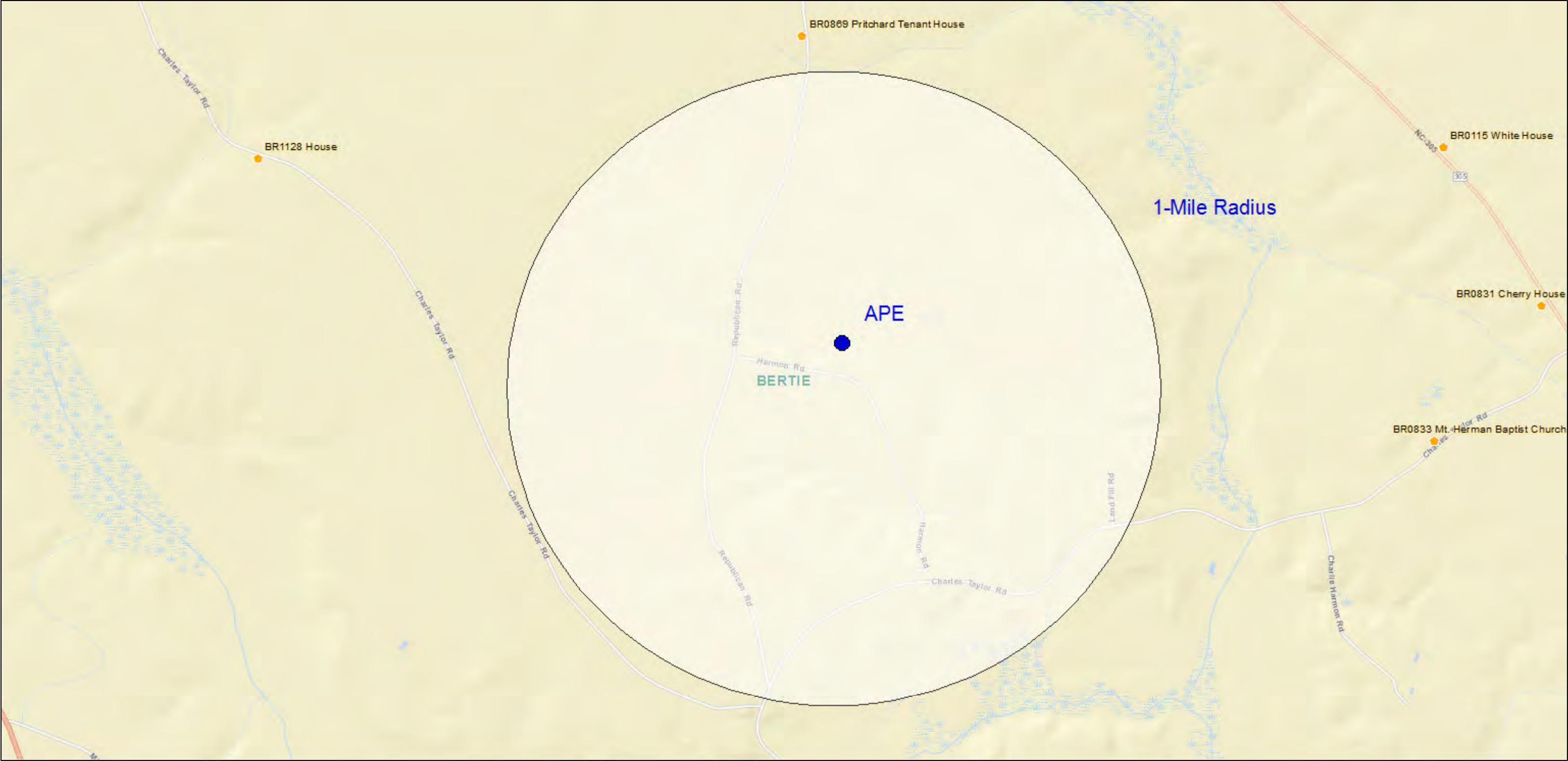
July 8, 2014

- | | | | | |
|--|--------------------------|--|------------------------------------|----------------------------------|
| ● NR Individual Listing | ▲ SL and DOE entry | □ Both SL and Determined Eligible Boundary | ★ SLDOEHD Center Point | □ Surveyed in NRHD, Gone |
| ● NR Listing, Gone | ■ Study List Entry, Gone | ▲ Determined Eligible | □ Determined Eligible Boundary | □ Blockface- Multiple properties |
| ★ NRHD Center Point | ▲ SL and DOE, Gone | ▲ DOE, Gone | □ Both DOE and Study List Boundary | □ Blockface in NRHD |
| □ National Register Boundary | ★ SLHD Center Point | ▲ SL and DOE | ▲ Surveyed Only | □ Surveyed Area, No designation |
| □ Boundary of Destroyed/Removed NR Listing | ★ SLDOEHD Center Point | ▲ SL and DOE, Gone | ▲ Surveyed in NRHD | □ County Boundaries |
| ■ SL Individual Entry | □ Study List Boundary | ★ DOEHD Center Point | ★ Surveyed Only, Gone | |



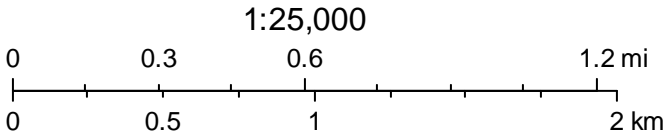
Sources: Esri, HERE, DeLorme, USGS, Intermap, increment P Corp., NRCAN, Esri Japan, METI, Esri China (Hong Kong), Esri (Thailand), TomTom, MapmyIndia, © OpenStreetMap contributors, and the GIS User Community

East Carolina Regional Landfill



July 8, 2014

- | | | | | |
|--|------------------------|--|----------------------------------|--------------------------------|
| NR Individual Listing | SL and DOE entry | Both SL and Determined Eligible Boundary | SLDOEHD Center Point | Surveyed in NRHD, Gone |
| NR Listing, Gone | Study List Entry, Gone | Determined Eligible | Determined Eligible Boundary | Blockface- Multiple properties |
| NRHD Center Point | SL and DOE, Gone | DOE, Gone | Both DOE and Study List Boundary | Blockface in NRHD |
| National Register Boundary | SLHD Center Point | SL and DOE | Surveyed Only | Surveyed Area, No designation |
| Boundary of Destroyed/Removed NR Listing | SLDOEHD Center Point | SL and DOE, Gone | Surveyed in NRHD | County Boundaries |
| SL Individual Entry | Study List Boundary | DOEHD Center Point | Surveyed Only, Gone | |



Sources: Esri, HERE, DeLorme, USGS, Intermap, increment P Corp., NRCAN, Esri Japan, METI, Esri China (Hong Kong), Esri (Thailand), TomTom, MapmyIndia, © OpenStreetMap contributors, and the GIS User Community

ATTACHMENT (F)

REFERENCES CITED

References Cited

North Carolina State Historic Preservation Office, Division of Historical Resources, 2014. HPOWEB GIS Map Service. Accessed online 30 June 2014 at <http://gis.ncdcr.gov/hpoweb/>

U.S. Coast Guard

n.d. Boat Station: Station Elizabeth City. Accessed online on 20 June 2014 at <http://www.uscg.mil/d5/staElizabethCity/>



North Carolina Department of Cultural Resources

State Historic Preservation Office

Ramona M. Bartos, Administrator

Governor Pat McCrory
Secretary Susan Kluttz

Office of Archives and History
Deputy Secretary Kevin Cherry

August 19, 2014

Jennifer Jones
AECOM
701 Corporate Center Drive, Suite 475
Raleigh, NC 27607

Re: Dredge At MWR Boat Ramp, US Coast Guard Base, Elizabeth City, Pasquotank County, ER 14-1676

Dear Ms. Jones:

Thank you for your letter of July 18, 2014, concerning the above project.

We have conducted a review of the project and are aware of no historic resources which would be affected by the project. Therefore, we have no comment on the project as proposed.

The above comments are made pursuant to Section 106 of the National Historic Preservation Act and the Advisory Council on Historic Preservation's Regulations for Compliance with Section 106 codified at 36 CFR Part 800.

Thank you for your cooperation and consideration. If you have questions concerning the above comment, contact Renee Gledhill-Earley, environmental review coordinator, at 919-807-6579 or renee.gledhill-earley@ncdcr.gov. In all future communication concerning this project, please cite the above referenced tracking number.

Sincerely,

A handwritten signature in blue ink that reads "Renee Gledhill-Earley".

for Ramona M. Bartos

Appendix F.

Permits

**U.S. ARMY CORPS OF ENGINEERS
WILMINGTON DISTRICT**

Action Id. SAW 2015-01189

County: Pasquotank

U.S.G.S. Quad: Elizabeth City

GENERAL PERMIT (REGIONAL AND NATIONWIDE) VERIFICATION

Property Owner: United States Coast Guard

c/o P. R. Carroll
Address: 1240 E. 9th Street
Cleveland, Ohio
44199
216-902-6200

Agent: Jennifer Jones

AECOM
701 Corporate Center Drive, Suite 475
Raleigh, NC
27607

Size and location of property (water body, road name/number, town, etc.): The property is located at the United States Coast Guard Base Elizabeth City, in the Pasquotank River, at 1664 Weeksville Road, Elizabeth City, Pasquotank County, North Carolina. (36.265496N, -76.173997)

Description of projects area and activity: The project involves the maintenance dredging of an approximate 65ft x 45ft man-made basin associated with the MWR boat ramp, to a maximum depth of 8 feet below MWL. Approximately 755 cubic yards will be dredged.

Applicable Law: ☐ Section 404 (Clean Water Act, 33 USC 1344)
☒ Section 10 (Rivers and Harbors Act, 33 USC 403)

Authorization: Regional General Permit Number or Nationwide Permit Number: GP 198200277

SEE ATTACHED NATIONWIDE AND SPECIAL CONDITIONS.

Your work is authorized by the above referenced permit provided it is accomplished in strict accordance with the attached conditions and your submitted application and attached information dated __. Any violation of the attached conditions or deviation from your submitted plans may subject the permittee to a stop work order, a restoration order and/or appropriate legal action.

This verification will remain valid until the expiration date identified below unless the nationwide authorization is modified, suspended or revoked. If, prior to the expiration date identified below, the nationwide permit authorization is reissued and/or modified, this verification will remain valid until the expiration date identified below, provided it complies with all requirements of the modified nationwide permit. If the nationwide permit authorization expires or is suspended, revoked, or is modified, such that the activity would no longer comply with the terms and conditions of the nationwide permit, activities which have commenced (i.e., are under construction) or are under contract to commence in reliance upon the nationwide permit, will remain authorized provided the activity is completed within twelve months of the date of the nationwide permit's expiration, modification or revocation, unless discretionary authority has been exercised on a case-by-case basis to modify, suspend or revoke the authorization.

Activities subject to Section 404 (as indicated above) may also require an individual Section 401 Water Quality Certification. You should contact the NC Division of Water Quality (telephone (919) 733-1786) to determine Section 401 requirements.

For activities occurring within the twenty coastal counties subject to regulation under the Coastal Area Management Act (CAMA), prior to beginning work you must contact the N.C. Division of Coastal Management in Washington, NC, at (252) 946-6481.

This Department of the Army verification does not relieve the permittee of the responsibility to obtain any other required Federal, State or local approvals/permits.

If there are any questions regarding this verification, any of the conditions of the Permit, or the Corps of Engineers regulatory program, please contact Josh Pelletier at (910) 251-4605.

Corps Regulatory Official 

Date: June 8, 2015

Expiration Date of Verification: December 31, 2017

The Wilmington District is committed to providing the highest level of support to the public. To help us ensure we continue to do so, please complete the attached customer Satisfaction Survey or visit <http://per2.nwp.usace.army.mil/survey.html> to complete the survey online.

Copy Furnished:

Determination of Jurisdiction:

- A. ☒ Based on preliminary information, there appear to be waters of the US including wetlands within the above described project area. This preliminary determination is not an appealable action under the Regulatory Program Administrative Appeal Process (Reference 33 CFR Part 331).
- B. ☐ There are Navigable Waters of the United States within the above described project area subject to the permit requirements of Section 10 of the Rivers and Harbors Act and Section 404 of the Clean Water Act. Unless there is a change in the law or our published regulations, this determination may be relied upon for a period not to exceed five years from the date of this notification.
- C. ☐ There are waters of the US and/or wetlands within the above described project area subject to the permit requirements of Section 404 of the Clean Water Act (CWA)(33 USC § 1344). Unless there is a change in the law or our published regulations, this determination may be relied upon for a period not to exceed five years from the date of this notification.
- D. ☐ The jurisdictional areas within the above described project area have been identified under a previous action. Please reference jurisdictional determination issued _____. Action ID _____.

Basis For Determination

The site is located in the Pasquotank River.

Remarks.

Attention USDA Program Participants

This delineation/determination has been conducted to identify the limits of Corps' Clean Water Act jurisdiction for the particular site identified in this request. The delineation/determination may not be valid for the wetland conservation provisions of the Food Security Act of 1985. If you or your tenant are USDA Program participants, or anticipate participation in USDA programs, you should request a certified wetland determination from the local office of the Natural Resources Conservation Service, prior to starting work.

F. Appeals Information (This information applies only to approved jurisdictional determinations as indicated in B and C above).

This correspondence constitutes an approved jurisdictional determination for the above described site. If you object to this determination, you may request an administrative appeal under Corps regulations at 33 CFR Part 331. Enclosed you will find a Notification of Appeal Process (NAP) fact sheet and request for appeal (RFA) form. If you request to appeal this determination you must submit a completed RFA form to the following address:

US Army Corps of Engineers
South Atlantic Division
Attn: Jason Steele, Review Officer
60 Forsyth Street SW, Room 10M15
Atlanta, Georgia 30303-8801
Phone: (404) 562-5137

In order for an RFA to be accepted by the Corps, the Corps must determine that it is complete, that it meets the criteria for appeal under 33 CFR part 331.5, and that it has been received by the Division Office within 60 days of the date of the NAP. Should you decide to submit an RFA form, it must be received at the above address by **August 8, 2015**.

****It is not necessary to submit an RFA form to the Division Office if you do not object to the determination in this correspondence.****

Corps Regulatory Official: _____

Date: **June 8, 2015**

Expiration Date: **December 31, 2017**

**NOTIFICATION OF ADMINISTRATIVE APPEAL OPTIONS AND PROCESS AND
REQUEST FOR APPEAL**

Applicant: **USCG, P.R. Carroll**

File Number: **SAW 2015-01189**

Date: **June 8, 2015**

Attached is:

See Section below

<input type="checkbox"/> INITIAL PROFFERED PERMIT (Standard Permit or Letter of permission)	A
<input type="checkbox"/> PROFFERED PERMIT (Standard Permit or Letter of permission)	B
<input type="checkbox"/> PERMIT DENIAL	C
<input type="checkbox"/> APPROVED JURISDICTIONAL DETERMINATION	D
<input checked="" type="checkbox"/> PRELIMINARY JURISDICTIONAL DETERMINATION	E

SECTION I - The following identifies your rights and options regarding an administrative appeal of the above decision. Additional information may be found at <http://www.usace.army.mil/inet/functions/cw/cecwo/reg> or Corps regulations at 33 CFR Part 331.

A: INITIAL PROFFERED PERMIT: You may accept or object to the permit.

- **ACCEPT:** If you received a Standard Permit, you may sign the permit document and return it to the district engineer for final authorization. If you received a Letter of Permission (LOP), you may accept the LOP and your work is authorized. Your signature on the Standard Permit or acceptance of the LOP means that you accept the permit in its entirety, and waive all rights to appeal the permit, including its terms and conditions, and approved jurisdictional determinations associated with the permit.
- **OBJECT:** If you object to the permit (Standard or LOP) because of certain terms and conditions therein, you may request that the permit be modified accordingly. You must complete Section II of this form and return the form to the district engineer. Your objections must be received by the district engineer within 60 days of the date of this notice, or you will forfeit your right to appeal the permit in the future. Upon receipt of your letter, the district engineer will evaluate your objections and may: (a) modify the permit to address all of your concerns, (b) modify the permit to address some of your objections, or (c) not modify the permit having determined that the permit should be issued as previously written. After evaluating your objections, the district engineer will send you a proffered permit for your reconsideration, as indicated in Section B below.

B: PROFFERED PERMIT: You may accept or appeal the permit

- **ACCEPT:** If you received a Standard Permit, you may sign the permit document and return it to the district engineer for final authorization. If you received a Letter of Permission (LOP), you may accept the LOP and your work is authorized. Your signature on the Standard Permit or acceptance of the LOP means that you accept the permit in its entirety, and waive all rights to appeal the permit, including its terms and conditions, and approved jurisdictional determinations associated with the permit.
- **APPEAL:** If you choose to decline the proffered permit (Standard or LOP) because of certain terms and conditions therein, you may appeal the declined permit under the Corps of Engineers Administrative Appeal Process by completing Section II of this form and sending the form to the division engineer. This form must be received by the division engineer within 60 days of the date of this notice.

C: PERMIT DENIAL: You may appeal the denial of a permit under the Corps of Engineers Administrative Appeal Process by completing Section II of this form and sending the form to the division engineer. This form must be received by the division engineer within 60 days of the date of this notice.

D: APPROVED JURISDICTIONAL DETERMINATION: You may accept or appeal the approved JD or provide new information.

- **ACCEPT:** You do not need to notify the Corps to accept an approved JD. Failure to notify the Corps within 60 days of the date of this notice, means that you accept the approved JD in its entirety, and waive all rights to appeal the approved JD.
- **APPEAL:** If you disagree with the approved JD, you may appeal the approved JD under the Corps of Engineers Administrative Appeal Process by completing Section II of this form and sending the form to the district engineer. This form must be received by the division engineer within 60 days of the date of this notice.

E: PRELIMINARY JURISDICTIONAL DETERMINATION: You do not need to respond to the Corps regarding the preliminary JD. The Preliminary JD is not appealable. If you wish, you may request an approved JD (which may be appealed), by contacting the Corps district for further instruction. Also you may provide new information for further consideration by the Corps to reevaluate the JD.

SECTION II - REQUEST FOR APPEAL or OBJECTIONS TO AN INITIAL PROFFERED PERMIT

REASONS FOR APPEAL OR OBJECTIONS: (Describe your reasons for appealing the decision or your objections to an initial proffered permit in clear concise statements. You may attach additional information to this form to clarify where your reasons or objections are addressed in the administrative record.)

ADDITIONAL INFORMATION: The appeal is limited to a review of the administrative record, the Corps memorandum for the record of the appeal conference or meeting, and any supplemental information that the review officer has determined is needed to clarify the administrative record. Neither the appellant nor the Corps may add new information or analyses to the record. However, you may provide additional information to clarify the location of information that is already in the administrative record.

POINT OF CONTACT FOR QUESTIONS OR INFORMATION:

If you have questions regarding this decision and/or the appeal process you may contact:

**District Engineer, Wilmington Regulatory Division,
Attn: Josh Pelletier**

Washington Regulatory Field Office
2407 West 5th Street
Washington, North Carolina 27889

If you only have questions regarding the appeal process you may also contact:

Mr. Jason Steele, Administrative Appeal Review Officer
CESAD-PDO
U.S. Army Corps of Engineers, South Atlantic Division
60 Forsyth Street, Room 10M15
Atlanta, Georgia 30303-8801
Phone: (404) 562-5137

RIGHT OF ENTRY: Your signature below grants the right of entry to Corps of Engineers personnel, and any government consultants, to conduct investigations of the project site during the course of the appeal process. You will be provided a 15 day notice of any site investigation, and will have the opportunity to participate in all site investigations.

Signature of appellant or agent.

Date: _____

Telephone number: _____

For appeals on Initial Proffered Permits send this form to:

District Engineer, Wilmington Regulatory Division, Attn: Josh Pelletier, 2407 West 5th Street, Washington, North Carolina 27889

For Permit denials, Proffered Permits and approved Jurisdictional Determinations send this form to:

**Division Engineer, Commander, U.S. Army Engineer Division, South Atlantic, Attn: Mr. Jason Steele, Administrative Appeal Officer, CESAD-PDO, 60 Forsyth Street, Room 10M15, Atlanta, Georgia 30303-8801
Phone: (404) 562-5137**

Action ID Number: SAW 2015-01189

County: Pasquotank

Permittee: USCG, c/o P.R. Carroll

Date Permit Issued: June 8, 2015

Project Manager: Josh Pelletier

Upon completion of the activity authorized by this permit and any mitigation required by the permit, sign this certification and return it to the following address:

**US ARMY CORPS OF ENGINEERS
WILMINGTON DISTRICT**

**Attn: Josh Pelletier
Washington Regulatory Field Office
2407 West 5th Street
Washington, North Carolina 27889**

Please note that your permitted activity is subject to a compliance inspection by a U. S. Army Corps of Engineers representative. If you fail to comply with this permit you are subject to permit suspension, modification, or revocation.

I hereby certify that the work authorized by the above referenced permit has been completed in accordance with the terms and condition of the said permit, and required mitigation was completed in accordance with the permit conditions.

Signature of Permittee

Date

DEPARTMENT OF THE ARMY
Wilmington District, Corps of Engineers
69 Darlington Avenue
Wilmington, North Carolina 28403-1343
<http://www.saw.usace.army.mil/WETLANDS/index.html>

General Permit No 198200277
Name of Permittee: General Public
Effective Date: January 1, 2011
Expiration Date: December 31, 2016

**DEPARTMENT OF THE ARMY
GENERAL (REGIONAL) PERMIT**

A general permit to perform work in or affecting navigable waters of the United States and waters of the United States, upon recommendation of the Chief of Engineers, pursuant to Section 10 of the Rivers and Harbors Act of March 3, 1899 (33 U.S.C. 403), and Section 404 of the Clean Water Act (33 U.S.C. 1344), is hereby renewed and modified by authority of the Secretary of the Army by the

District Engineer
U.S. Army Engineer District, Wilmington
Corps of Engineers
69 Darlington Avenue
Wilmington, North Carolina 28403-1343

TO AUTHORIZE WITHIN THE STATE OF NORTH CAROLINA, THE DREDGING AND THE DISCHARGE OF DREDGED OR FILL MATERIAL INCLUDING THE CONSTRUCTION, BACKFILLING AND MAINTENANCE OF BULKHEADS AND RIPRAP WITHIN MANMADE BASINS AND CANALS LOCATED ENTIRELY IN UPLANDS AND TO AUTHORIZE MAINTENANCE DREDGING OF DITCHES, BASINS, CANALS AND CHANNELS IN OPEN WATER AREAS, WHERE ALL EXCAVATED MATERIAL IS PLACED AND RETAINED ENTIRELY ON HIGH GROUND.

Special Conditions

- a. Any excavation authorized by this general permit, new or maintenance, may involve no more than 1,000 cubic yards of material as part of a single and complete project.
- b. This general permit does not authorize the excavation of or discharge into Department of the Army jurisdictional wetlands, seagrasses or other submerged aquatic vegetation. Excavation near wetlands must allow for an adequate buffer (not less than 15 feet) between the

excavated area and the wetlands to avoid erosion of the wetlands.

c. Bulkhead and riprap placement must not exceed an average of 5 feet nor a maximum 10 feet waterward of the mean high water mark (MHW) in tidal waters or the ordinary high water mark (OHWM) or ordinary high water elevation contour in non-tidal waters.

d. This permit does not authorize the placement of bulkheads or riprap material waterward of jurisdictional wetlands, seagrasses, or other SAV's.

e. All excavated material must be placed and retained entirely on high ground landward of the Corps of Engineers' regulatory jurisdiction and must be retained and stabilized to prevent any material from reentering jurisdictional areas.

f. This general permit does not authorize the excavation or discharge of material which contains toxic pollutants in toxic amounts as identified pursuant to Clean Water Act Section 307. Prior to commencing work the permittee must review all existing and readily available information to ensure, to the extent practicable, that such pollutants are not present.

g. Maintenance excavation must not exceed the original dimensions as defined by the original permit or as clearly shown or otherwise defined by existing conditions of the ditches, basins, canals, or channels. Maintenance excavation is allowed only for facilities that are currently serviceable.

i. The excavation depth in inland canals or basins, located entirely in high ground, is limited to the more shallow of the depth of the connecting canals/channels or six (6) feet below mean or ordinary low water.

j. No excavation or filling may occur during applicable moratorium times as designated by the North Carolina Division of Coastal Management or the North Carolina Wildlife Resources Commission for protection of fish and/or shellfish.

k. This general permit does not authorize excavation or filling in any area designated by the North Carolina Division of Marine Fisheries and/or the North Carolina Wildlife Resources Commission as primary nursery area or prime shellfish area.

l. Should all or part of a proposed development activity be located in an Area of Environmental Concern (AEC) as designated by the North Carolina Coastal Resources Commission, a Coastal Area Management Act (CAMA) permit is required from the North Carolina Division of Coastal Management before the onset of the proposed activity. Should a Federal activity within any one of North Carolina's twenty coastal counties or which could affect a coastal use or resource in any one of North Carolina's twenty coastal counties be proposed by a Federal agency, a consistency determination pursuant to Subpart "C" of 15 CFR 930 must be prepared and submitted by that Federal agency to the North Carolina Division of Coastal Management before the onset of the proposed activity.

m. Activities in any mountain trout waters must comply with all pH, temperature and turbidity criteria established for such waters by the North Carolina Wildlife Resources Commission and/or the North Carolina Division of Water Quality. Work that may result in the sedimentation of trout waters will generally be prohibited from November 1 to April 15, of any year, to avoid impacts on trout spawning.

n. Permittees shall obtain a letter of approval from the North Carolina Wildlife Resources Commission and furnish a copy of the letter to the Wilmington District Corps of Engineers, prior to discharging dredged or fill material into waters of the United States, including wetlands, in the twenty-five (25) mountain counties of North Carolina that contain trout waters. To obtain this approval, applicants should contact:

North Carolina Wildlife Resources Commission
Habitat Conservation Program Manager
512 North Salisbury Street
Raleigh, North Carolina 27611
Telephone (919) 733-7638

The counties in which this condition applies are:

Alleghany	Ashe	Avery
Buncombe	Burke	Caldwell
Cherokee	Clay	Graham
Haywood	Henderson	Jackson
Macon	Madison	McDowell
Mitchell	Polk	Rutherford
Stokes	Surry	Swain
Transylvania	Watauga	Wilkes
Yancey		

General Conditions.

a. Except as authorized by this general permit or any USACE approved modification to this general permit, no excavation, fill or mechanized land-clearing activities shall take place within waters or wetlands, at any time in the construction or maintenance of this project. This permit does not authorize temporary placement or double handling of excavated or fill material within waters or wetlands outside the permitted area. This prohibition applies to all borrow and fill activities connected with this project.

b. Authorization under this general permit does not obviate the need to obtain other federal, state, or local authorizations.

c. All work authorized by this general permit must comply with the terms and conditions of the applicable Clean Water Act Section 401 Water Quality Certification for this general permit issued by the North Carolina Division of Water Quality.

d. The permittee shall employ all sedimentation and erosion control measures necessary to prevent an increase in sedimentation or turbidity within waters and wetlands outside the permit area. This shall include, but is not limited to, the immediate installation of silt fencing or similar appropriate devices around all areas subject to soil disturbance or the movement of earthen fill, and the immediate stabilization of all disturbed areas. Additionally, the project must remain in full compliance with all aspects of the Sedimentation Pollution Control Act of 1973 (North Carolina General Statutes Chapter 113A Article 4).

e. The activities authorized by this general permit must not interfere with the public's right to free navigation on all navigable waters of the United States. No attempt will be made by the permittee to prevent the full and free use by the public of all navigable waters at or adjacent to the authorized work for a reason other than safety.

f. The permittee understands and agrees that, if future operations by the United States require the removal, relocation, or other alteration, of the structure or work herein authorized, or if, in the opinion of the Secretary of the Army or his authorized representative, said structure or work shall cause unreasonable obstruction to the free navigation of the navigable waters, the permittee will be required, upon due notice from the Corps of Engineers, to remove, relocate, or alter the structural work or obstructions caused thereby, without expense to the United States. No claim shall be made against the United States on account of any such removal or alteration.

g. The permittee, upon receipt of a notice of revocation of this permit or upon its expiration before completion of the work will, without expense to the United States and in such time and manner as the Secretary of the Army or his authorized representative may direct, restore the affected water of the United States to its former conditions.

h. The permittee will allow the Wilmington District Engineer or his representative to inspect the authorized activity at any time deemed necessary to assure that the activity is being performed or maintained in strict accordance with the Special and General Conditions of this permit.

i. This general permit does not grant any property rights or exclusive privileges.

j. This permit does not authorize any injury to the property or rights of others.

k. This general permit does not authorize the interference with any existing or proposed federal project.

1. In issuing this permit, the Federal Government does not assume any liability for the following:

(1) Damages to the permitted project or uses thereof as a result of other permitted or unpermitted activities or from natural causes.

(2) Damages to the permitted project or uses thereof as a result of current or future activities undertaken by or on behalf of the United States in the public interest.

(3) Damages to persons, property, or to other permitted or unpermitted activities or structures caused by the activity authorized by this permit.

(4) Design or construction deficiencies associated with the permitted work

(5) Damage claims associated with any future modification, suspension, or revocation of this permit.

m. Authorization provided by this general permit may be modified, suspended or revoked in whole or in part if the Wilmington District Engineer, acting for the Secretary of the Army, determines that such action would be in the best public interest. The term of this general permit shall be five (5) years unless subject to modification, suspension or revocation. Any modification, suspension or revocation of this authorization will not be the basis for any claim for damages against the United States Government.

n. This general permit does not authorize any activity, which the District Engineer determines, after any necessary investigations, would adversely affect:

(1) Rivers named in Section 3 of the Wild and Scenic Rivers Act (15 U.S.C. 1273), those proposed for inclusion as provided by Sections 4 and 5 of the Act and wild, scenic and recreational rivers established by state and local entities.

(2) Historic, cultural or archeological sites listed in or eligible for inclusion in the National Register of Historic Places as defined in the National Historic Preservation Act of 1966 and its codified regulations, the National Historic Preservation Amendment Acts of 1980 and 1992, the Abandoned Shipwreck Act of 1987 and the Native American Graves Protection and Repatriation Act.

(3) Sites included in or determined eligible for listing in the National Registry of Natural Landmarks.

o. This general permit does not authorize any activity, which will adversely affect any threatened or endangered species or a species proposed for such designation, or their designated critical habitat as identified under the Federal Endangered Species Act (16 U.S.C. 1531).

Information on the location of threatened and endangered species and their critical habitat can be obtained directly from the Corps field offices or at the following internet address: <http://www.ncnhp.org/Pages/heritagedata.html>. or <http://nc-es.fws.gov/es/es.html>. Permittees should notify the Corps if any listed species or designated critical habitat might be affected by the proposed project and may not begin work until notified by the Corps that the requirements of the Endangered Species Act have been satisfied and that the activity is authorized.

p. Permittees are advised that development activities in or near a floodway may be subject to the National Flood Insurance Program that prohibits any development, including fill, within a floodway that results in any increase in base flood elevations. This general permit does not authorize any activity prohibited by the National Flood Insurance Program.

q. The permittee must install and maintain, at his expense, any signal lights and signals prescribed by the U.S. Coast Guard, through regulations or otherwise, on authorized facilities. For further information, the permittee should contact the U.S. Coast Guard Marine Safety Office at (910) 772-2191.

r. The permittee must maintain any structure or work authorized by this permit in good condition and in conformance with the terms and conditions of this permit. The Permittee is not relieved of this requirement if the Permittee abandons the structure or work. Transfer in fee simple of the work authorized by this permit will automatically transfer this permit to the property's new owner, with all of the rights and responsibilities enumerated herein. The permittee must inform any subsequent owner of all activities undertaken under the authority of this permit and provide the subsequent owner with a copy of the terms and conditions of this permit.

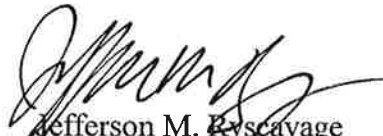
s. At his sole discretion, any time during the processing cycle, the Wilmington District Engineer may determine that this general permit will not be applicable to a specific proposal. In such case, the procedures for processing an individual permit in accordance with 33 CFR 325 will be available.

t. Except as authorized by this general permit or any USACE approved modification to this general permit, all fill material placed in waters or wetlands shall be generated from an upland source and will be clean and free of any pollutants except in trace quantities. Metal products, organic materials (including debris from land clearing activities), or unsightly debris will not be used.

u. Except as authorized by this general permit or any USACE approved modification to this general permit, all excavated material will be disposed of in approved upland disposal areas.

v. Activities which have commenced (i.e., are under construction) or are under contract to commence in reliance upon this general permit will remain authorized provided the activity is completed within twelve months of the date of the general permit's expiration, modification, or revocation. Activities completed under the authorization of this general permit which were in effect at the time the activity was completed continue to be authorized by the general permit.

BY AUTHORITY OF THE SECRETARY OF THE ARMY:



Jefferson M. Ryscavage
Colonel, U.S. Army
District Commander



North Carolina Department of Environment and Natural Resources

Pat McCrory
Governor

Donald R. van der Vaart
Secretary

April 30, 2015

DWR # 14-1039
Pasquotank County

United States Coast Guard, Civil Engineering Unit Cleveland
CDR Pete Carroll
1240 East Ninth Street, Room 2179
Cleveland, Ohio 44199-2060

**Subject: APPROVAL OF 401 WATER QUALITY CERTIFICATION WITH ADDITIONAL
CONDITIONS**

**Project: USCG Base Elizabeth City – Dredging at MWR Boat Ramp
Pasquotank River Basin, Pasquotank River [PAS 50, 30-3-(12); SB]**

Dear Commander Carroll:

You have our approval for the impacts listed below for the purpose described in your application dated September 3, 2014, received by the Division of Water Resources (Division) September 26, 2014. These impacts are covered by the attached Water Quality General Certification Number 3894 (GC 3894). This General Certification(s) allows you to use Regional General Permit(s) 198200277 when issued by the US Army Corps of Engineers (USACE). Please note that you should get any other federal, state or local permits before proceeding with your project, including those required by (but not limited to) Sediment and Erosion Control, Non-Discharge, and Water Supply Watershed regulations. This project has been on Hold while it went through the NEPA process and was reported issued a Finding of No Significant Impact (FONSI).

This approval requires you to follow the conditions listed in the enclosed certification or general permit and the following additional conditions:

1. The following impacts are hereby approved provided that all of the other specific and general conditions of the Certification are met. No other impacts are approved, including incidental impacts.

Type of Impact	Amount Approved (units) Permanent	Amount Approved (units) Temporary
Stream		
Site 1	(linear feet)	
404/401 Wetlands		
Site 1	(acres)	(acres)
Coastal Wetlands		
Site 1	(acres)	(acres)
Open Waters		
Site 1	0.18 (acres) Dredge	(acres)
Buffers – Zone 1		
Site 1	(square feet)	(square feet)
Buffers – Zone 2		
Site 1	(square feet)	(square feet)

2. This approval is for the purpose and design described in your application. The plans and specifications for this project are incorporated by reference as part of the Certification/Authorization. If you change your project, you must notify the Division and you may be required to submit a new application package with the appropriate fee. If the property is sold, the new owner must be given a copy of this approval letter and General Certification(s) and is responsible for complying with all conditions.

Certificate of Completion

Upon completion of all work approved within the 401 Water Quality Certification or applicable Buffer Rules, and any subsequent modifications, the applicant is required to return the attached certificate of completion to the 401 Oversight/Express Review Permitting Unit, North Carolina Division of Water Quality, 1650 Mail Service Center, Raleigh, NC, 27699-1650.

This approval and its conditions are final and binding unless contested.

This Certification can be contested as provided in Articles 3 and 4 of General Statute 150B by filing a written petition for an administrative hearing to the Office of Administrative Hearings (hereby known as OAH) **within sixty(60) calendar days**.

A petition form may be obtained from the OAH at <http://www.ncoah.com/> or by calling the OAH Clerk's Office at (919) 431-3000 for information. A petition is considered filed when the original and one (1) copy along with any applicable OAH filing fee is received in the OAH during

normal office hours (Monday through Friday between 8:00am and 5:00pm, excluding official state holidays). The petition may be faxed to the OAH at (919) 431-3100, provided the original and one copy of the petition along with any applicable OAH filing fee is received by the OAH within five (5) business days following the faxed transmission.

Mailing address for the OAH:

If sending via US Postal Service:

Office of Administrative Hearings
6714 Mail Service Center
Raleigh, NC 27699-6714

If sending via delivery service (UPS, FedEx, etc):

Office of Administrative Hearings
1711 New Hope Church Road
Raleigh, NC 27609-6285

One (1) copy of the petition must also be served to DENR:

Lacy Presnell, General Counsel
Department of Environment and Natural Resources
1601 Mail Service Center
Raleigh, NC 27699-1601

This letter completes the review of the Division under section 401 of the Clean Water Act. Please contact Roberto Scheller by phone at 252-948-3940 or by email at roberto.scheller@ncdenr.gov if you have any questions or concerns.

Sincerely,



Robert Tankard
Assistant Regional Supervisor
Division of Water Resources

Enclosures: GC 3894
Certification of Completion

cc: Jennifer Jones, AECOM, email: Jennifer.Jones@aecom.com
Chet Bigelow, USACE, Washington Regulatory Field Office
DWR401 & Buffer Permitting Unit
File

Water Quality Certification No. 3894

**GENERAL CERTIFICATION FOR PROJECTS ELIGIBLE FOR U.S. ARMY CORPS
OF ENGINEERS NATIONWIDE PERMIT 35 (MAINTENANCE DREDGING
OF EXISTING BASINS) AND REGIONAL (GENERAL) PERMIT NUMBERS 198200277 AND
199602878 (CANALS AND BOAT BASINS IN HIGH GROUND AND MAINTENANCE
DREDGING IN OPEN WATERS)
AND RIPARIAN AREA PROTECTION RULES (BUFFER RULES)**

Water Quality Certification Number 3894 is issued in conformity with the requirements of Section 401, Public Laws 92-500 and 95-217 of the United States and subject to the North Carolina Division of Water Quality Regulations in 15A NCAC 02H .0500 and 15A NCAC 02B .0200 for the discharge of fill material to waters and wetland areas as described in 33 CFR 330 Appendix A (B) (35) Regional (General) Permit Number 198200277 and 199602878 and for the Riparian Area Protection Rules (Buffer Rules) in 15A NCAC 02B .0200.

The State of North Carolina certifies that the specified category of activity will not violate applicable portions of Sections 301, 302, 303, 306 and 307 of the Public Laws 92-500 and 95-217 if conducted in accordance with the conditions hereinafter set forth.

Activities meeting any one (1) of the following thresholds or circumstances require written approval for a 401 Water Quality Certification from the Division of Water Quality (the "Division).

- a) Any proposed fill or modification of wetlands and/or waters, including streams; or
- b) Any stream relocation; or
- c) Any impact associated with a Notice of Violation or an enforcement action for violation(s) of DWQ Wetland Rules (15A NCAC 02H .0500), Isolated Wetland Rules (15A NCAC 02H .1300), DWQ Surface Water or Wetland Standards, or Riparian Buffer Rules (15A NCAC 02B .0200); or
- d) Any impacts to streams and/or buffers in the Neuse, Tar-Pamlico, or Catawba River Basins or in the Randleman, Jordan or Goose Creek Watersheds (or any other basin or watershed with Riparian Area Protection Rules [Buffer Rules] in effect at the time of application) *unless* the activities are listed as "EXEMPT" from these rules or a Buffer Authorization Certificate is issued through N.C. Division of Coastal Management (DCM) delegation for "ALLOWABLE" activities.

In accordance with North Carolina General Statute 143-215.3D(e), written approval for a 401 Water Quality General Certification must include the appropriate fee. If a project also requires a CAMA Permit, then one payment to both agencies shall be submitted and will be the higher of the two fees.

Activities included in this General Certification that do not meet one of the thresholds listed above do not require written approval from the Division as long as they comply with the Conditions of Certification listed below. If any of these Conditions cannot be met, then written approval from the Division is required.

Conditions of Certification:

1. Excavation and/or filling of wetlands is not authorized by this General Certification.
2. No Impacts Beyond those Authorized in the Written Approval or Beyond the Threshold of Use of this Certification

No waste, spoil, solids, or fill of any kind shall occur in wetlands, waters, or riparian areas beyond the footprint of the impacts depicted in the Pre-Construction Notification, as authorized in the written approval from the Division or beyond the thresholds established for use of this Certification without written authorization, including incidental impacts. All

Water Quality Certification No. 3894

construction activities, including the design, installation, operation, and maintenance of sediment and erosion control Best Management Practices shall be performed so that no violations of state water quality standards, statutes, or rules occur. Approved plans and specifications for this project are incorporated by reference and are enforceable parts of this permit.

3. Standard Erosion and Sediment Control Practices

Erosion and sediment control practices must be in full compliance with all specifications governing the proper design, installation and operation and maintenance of such Best Management Practices and if applicable, comply with the specific conditions and requirements of the NPDES Construction Stormwater Permit issued to the site:

- a. Design, installation, operation, and maintenance of the sediment and erosion control measures must be such that they equal or exceed the requirements specified in the most recent version of the *North Carolina Sediment and Erosion Control Manual*. The devices shall be maintained on all construction sites, borrow sites, and waste pile (spoil) projects, including contractor-owned or leased borrow pits associated with the project.
- b. For borrow pit sites, the erosion and sediment control measures must be designed, installed, operated, and maintained in accordance with the most recent version of the *North Carolina Surface Mining Manual*.
- c. Reclamation measures and implementation must comply with the reclamation in accordance with the requirements of the Sedimentation Pollution Control Act and the Mining Act of 1971.
- d. Sufficient materials required for stabilization and/or repair of erosion control measures and stormwater routing and treatment shall be on site at all times.
- e. If the project occurs in waters or watersheds classified as Primary Nursery Areas (PNAs), SA, WS-I, WS-II, High Quality (HQW), or Outstanding Resource (ORW) waters, then the sedimentation and erosion control designs must comply with the requirements set forth in 15A NCAC 04B .0124, *Design Standards in Sensitive Watersheds*.

4. No Sediment and Erosion Control Measures in Wetlands or Waters

Sediment and erosion control measures shall not be placed in wetlands or waters. Exceptions to this condition require application submittal to and written approval by the Division. If placement of sediment and erosion control devices in wetlands and waters is unavoidable, then design and placement of temporary erosion control measures shall not be conducted in a manner that may result in dis-equilibrium of wetlands, stream beds, or banks, adjacent to or upstream and downstream of the above structures. All sediment and erosion control devices shall be removed and the natural grade restored within two (2) months of the date that the Division of Land Resources (DLR) or locally delegated program has released the specific area within the project.

Water Quality Certification No. 3894

5. Construction Stormwater Permit NCG010000

An NPDES Construction Stormwater Permit is required for construction projects that disturb one (1) or more acres of land. This Permit allows stormwater to be discharged during land disturbing construction activities as stipulated in the conditions of the permit. If your project is covered by this permit, full compliance with permit conditions including the erosion & sedimentation control plan, inspections and maintenance, self-monitoring, record keeping and reporting requirements is required. A copy of the general permit (NCG010000), inspection log sheets, and other information may be found at <http://portal.ncdenr.org/web/wq/ws/su/npdessw#tab-w>.

The North Carolina Department of Transportation (NCDOT) shall be required to be in full compliance with the conditions related to construction activities within the most recent version of their individual NPDES (NCS000250) stormwater permit.

6. Construction Moratoriums and Coordination

If activities must occur during periods of high biological activity (i.e. sea turtle nesting, fish spawning, or bird nesting), then biological monitoring may be required at the request of other state or federal agencies and coordinated with these activities.

All moratoriums on construction activities established by the NC Wildlife Resources Commission (WRC), US Fish and Wildlife Service (USFWS), NC Division of Marine Fisheries (DMF), or National Marine Fisheries Service (NMFS) to lessen impacts on trout, anadromous fish, larval/post-larval fishes and crustaceans, or other aquatic species of concern shall be implemented. Exceptions to this condition require written approval by the resource agency responsible for the given moratorium.

Work within the twenty-five (25) designated trout counties or identified state or federal endangered or threatened species habitat shall be coordinated with the appropriate WRC, USFWS, NMFS, and/or DMF personnel.

7. Work in the Dry

All work in or adjacent to stream waters shall be conducted so that the flowing stream does not come in contact with the disturbed area. Approved best management practices from the most current version of the NC Sediment and Erosion Control Manual, or the NC DOT Construction and Maintenance Activities Manual, such as sandbags, rock berms, cofferdams, and other diversion structures shall be used to minimize excavation in flowing water. Exceptions to this condition require application submittal to and written approval by the Division.

8. Riparian Area Protection (Buffer) Rules

Activities located in the protected riparian areas (whether jurisdictional wetlands or not), within the Neuse, Tar-Pamlico, or Catawba River Basins or in the Randleman, Jordan, or Goose Creek Watersheds (or any other basin or watershed with buffer rules) shall be limited to "uses" identified within and constructed in accordance with 15A NCAC 02B .0233, .0259, .0243, .0250, .0267 and .0605, and shall be located, designed, constructed, and maintained to have minimal disturbance to protect water quality to the maximum extent practicable through the use of best management practices. All buffer rule requirements, including diffuse flow requirements, must be met.

9. If concrete is used during the construction, then all necessary measures shall be taken to prevent direct contact between uncured or curing concrete and waters of the state. Water

Water Quality Certification No. 3894

that inadvertently contacts uncured concrete shall not be discharged to waters of the state due to the potential for elevated pH and possible aquatic life/ fish kills.

10. Compensatory Mitigation

In accordance with 15A NCAC 02H .0506 (h), compensatory mitigation may be required for losses of equal to or greater than 150 linear feet of streams (intermittent and perennial) and/or equal to or greater than one (1) acre of wetlands. For linear public transportation projects, impacts equal to or exceeding 150 linear feet per stream shall require mitigation.

Buffer mitigation may be required for any project with Buffer Rules in effect at the time of application for activities classified as "Allowable with Mitigation" or "Prohibited" within the Table of Uses.

A determination of buffer, wetland, and stream mitigation requirements shall be made for any General Water Quality Certification for this Nationwide and/or Regional General Permit. Design and monitoring protocols shall follow the US Army Corps of Engineers Wilmington District *Stream Mitigation Guidelines* (April 2003) or its subsequent updates. Compensatory mitigation plans shall be submitted to the Division for written approval as required in those protocols. The mitigation plan must be implemented and/or constructed before any impacts occur on site. Alternatively, the Division will accept payment into an in-lieu fee program or a mitigation bank. In these cases, proof of payment shall be provided to the Division before any impacts occur on site.

11. The canal or boat basin shall be allowed to be dredged to its original depth and width. The applicant is required to supply information regarding the original depth and width of the site. The area shall not be dredged to a depth deeper than the receiving water.
12. All fill material shall be placed on high ground and be from an upland source, and excavation shall not occur in Primary Nursery Areas.
13. If an environmental document is required under the National or State Environmental Policy Act (NEPA or SEPA), then this General Certification is not valid until a Finding of No Significant Impact (FONSI) or Record of Decision (ROD) is issued by the State Clearinghouse.
14. In the twenty (20) coastal counties, the appropriate DWQ Regional Office must be contacted to determine if Coastal Stormwater Regulations will be required.
15. This General Certification does not relieve the applicant of the responsibility to obtain all other required Federal, State, or Local approvals.
16. The applicant/permittee and their authorized agents shall conduct all activities in a manner consistent with State water quality standards (including any requirements resulting from compliance with §303(d) of the Clean Water Act), and any other appropriate requirements of State and Federal Law. If the Division determines that such standards or laws are not being met, including failure to sustain a designated or achieved use, or that State or Federal law is being violated, or that further conditions are necessary to assure compliance, then the Division may reevaluate and modify this General Water Quality Certification.
17. When written authorization is required for use of this certification, upon completion of all permitted impacts included within the approval and any subsequent modifications, the applicant shall be required to return the certificate of completion attached to the approval. One copy of the certificate shall be sent to the DWQ Central Office in Raleigh at 1650 Mail Service Center, Raleigh, NC, 27699-1650.

Water Quality Certification No. 3894

18. Additional site-specific conditions, including monitoring and/or modeling requirements, may be added to the written approval letter for projects proposed under this Water Quality Certification in order to ensure compliance with all applicable water quality and effluent standards.
19. This certification grants permission to the director, an authorized representative of the Director, or DENR staff, upon the presentation of proper credentials, to enter the property during normal business hours.

This General Certification shall expire on the same day as the expiration date of the corresponding Nationwide and/or Regional General Permit. The conditions in effect on the date of issuance of Certification for a specific project shall remain in effect for the life of the project, regardless of the expiration date of this Certification.

Non-compliance with or violation of the conditions herein set forth by a specific project may result in revocation of this General Certification for the project and may also result in criminal and/or civil penalties.

The Director of the North Carolina Division of Water Quality may require submission of a formal application for Individual Certification for any project in this category of activity if it is determined that the project is likely to have a significant adverse effect upon water quality, including state or federally listed endangered or threatened aquatic species, or degrade the waters so that existing uses of the wetland or downstream waters are precluded.

Public hearings may be held for specific applications or group of applications prior to a Certification decision if deemed in the public's best interest by the Director of the North Carolina Division of Water Quality.

Effective date: March 19, 2012

DIVISION OF WATER QUALITY

By

A handwritten signature in blue ink, appearing to read "Matt M..." followed by a flourish.

Charles Wakild, P.E.

Director

History Note: Water Quality Certification (WQC) Number 3894 issued March 19, 2012, replaces WQC Number 3698 issued November 1, 2007; WQC Number 3635 issued March 19, 2007; WQC Number 3489 issued December 31, 2004; and WQC 3349 issued on March 18, 2002. This General Certification is rescinded when the Corps of Engineers reauthorizes any of the corresponding Nationwide and/or Regional General Permits or when deemed appropriate by the Director of the Division of Water Quality.



North Carolina Department of Environment and Natural Resources

Pat McCrory
Governor

Donald R. van der Vaart
Secretary

DWR Project No: _____ County: _____

Applicant: _____

Project Name: _____

401 Water Quality Certification Issued Date: _____

Certificate of Completion

Upon completion of all work approved within the 401 Water Quality Certification or applicable Buffer Rules, and any subsequent modifications, the applicant is required to return this certificate to the DWR 401 & Buffer Permitting Unit, 1650 Mail Service Center, Raleigh, NC, 27699-1650. This form may be returned to DWR by the applicant, the applicant's authorized agent, **or** the project engineer. It is not necessary to send certificates from all of these.

Applicant's Certification

I, _____, hereby state that, to the best of my abilities, due care and diligence was used in the observation of the construction such that the construction was observed to be built within substantial compliance and intent of the 401 Water Quality Certification and Buffer Rules, the approved plans and specifications, and other supporting materials.

Signature: _____ Date: _____

Agent's Certification

I, _____, hereby state that, to the best of my abilities, due care and diligence was used in the observation of the construction such that the construction was observed to be built within substantial compliance and intent of the 401 Water Quality Certification and Buffer Rules, the approved plans and specifications, and other supporting materials.

Signature: _____ Date: _____

If this project was designed by a Certified Professional

I, _____, as a duly registered Professional _____ (i.e., Engineer, Landscape Architect, Surveyor, etc.) in the State of North Carolina, having been authorized to observe (periodically, weekly, full time) the construction of the project, for the Permittee hereby state that, to the best of my abilities, due care and diligence was used in the observation of the construction such that the construction was observed to be built within substantial compliance and intent of the 401 Water Quality Certification and Buffer Rules, the approved plans and specifications, and other supporting materials.

Signature: _____ Date: _____



Republic Services, Inc.

18500 N. Allied Way, Phoenix, AZ 85054

SPECIAL WASTE DEPARTMENT DECISION

Waste Profile #
30261412720

Expiration Date
7/30/2016

I. Decision Request:

☐ Initial ☐ Recertification ☒ Change

Disposal Facility: 3026 - East Carolina Regional Landfill

Generator Name: USCG Base Elizabeth City

Generator Site Address: 1664 Weeksville Road

City: Elizabeth City

County:

State: NC

Zip:

Name of Waste: Dredge Material

Estimated Annual Volume: 755 Cubic Yards

II. Special Waste Department Decision:

☒ Approved ☐ Rejected

Management Method(s):

☒ Landfill ☐ Solidification ☐ Bioremediation ☐ Transfer Facility

Problematic Special Waste according to Republic?

☐ Yes ☒ No

If yes, which one?

Approved by Special Waste Review Committee?

☐ Yes ☐ No ☒ Not Applicable

Precautions, Conditions or Limitations on Approval

Per the Special Waste Change Form dated 6-15-2015, the expiration date was extended to allow for disposal of the waste profiled.

Special Waste Analyst Signature: _____

Date: 6/16/2015

Name (Printed): Suzanne Glass

III. Facility Decision:

☐ Approved ☐ Rejected

Precautions, Conditions or Limitations on Approval

By signing below, the General Manager or Designee agrees that a fully executed Special Waste Service Agreement is on file for this profile and that the special waste file is complete.

General Manager or Designee: _____

Date: 6/16/2015

Name (Printed): _____



SPECIAL WASTE PROFILE – CHANGE

Saveable fill-in form. Restricted printing until all required (yellow) fields are completed.

I. Generator Information

This form may be used to request changes to an existing Special Waste Profile.			
Generator Name:	USCG Base Elizabeth City		
Name of Waste:	Dredge Material	Waste Profile #	30261412720

II. Purpose of Change


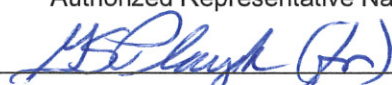
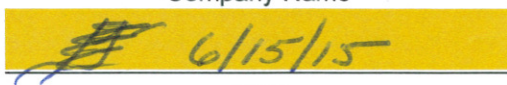
Description of Change Requested and Reason for Change: (Provide detailed explanation of why the change is requested following the appropriate checked box below).	
<input type="checkbox"/> Volume Increase By:	Is the analysis originally submitted with the Profile representative of the volume Increase? <input type="checkbox"/> Yes <input type="checkbox"/> No If No, complete Section III, below.
<input checked="" type="checkbox"/> Extend Expiration Date: Extend Expiration date for another 12 months	
<input type="checkbox"/> Change or Add Landfill:	
<input type="checkbox"/> Add Additional Laboratory Reports: Complete Representative Sample Certification, Section III, below.	
<input type="checkbox"/> Add MSDS:	
<input type="checkbox"/> Generator Name Change:	
<input checked="" type="checkbox"/> Other: Project was suspended and then further delayed due to a fish hatching moratorium in which dredging activities could not commence. Requesting extension to the original expiration date due to this delay. There has been no change to the material/characteristics.	

III. Representative Sample Certification

☒ No Sample Taken

Is the representative sample collected to prepare this profile and laboratory analysis, collected in accordance with U.S. EPA 40 CFR 261.20(c) guidelines or equivalent rules?	<input type="checkbox"/> YES or <input type="checkbox"/> NO
Type of Sample: <input type="checkbox"/> COMPOSITE SAMPLE <input type="checkbox"/> GRAB SAMPLE	
Sample Date:	
Sample ID Numbers:	

IV. Certification

I hereby certify that the waste and the process generating the waste are unchanged and are accurately represented in the original profile.	
Peter R. Carroll, CDR	U.S. Coast Guard Civil Engineering Unit 
Authorized Representative Name and Title (Printed)	Company Name
	
Authorized Representative Signature	Date



Republic Services, Inc.

18500 N. Allied Way, Phoenix, AZ 85054

SPECIAL WASTE DEPARTMENT DECISION

Waste Profile #
30261412720

Expiration Date
7/30/2015

I. Decision Request:

☒ Initial ☐ Recertification ☐ Change

Disposal Facility: 3026 - East Carolina Regional Landfill

Generator Name: USCG Base Elizabeth City

Generator Site Address: 1664 Weeksville Road

City: Elizabeth City

County:

State: NC

Zip:

Name of Waste: Dredge Material

Estimated Annual Volume: 755 Cubic Yards

II. Special Waste Department Decision:

☒ Approved ☐ Rejected

Management Method(s):

☒ Landfill ☐ Solidification ☐ Bioremediation ☐ Transfer Facility

Problematic Special Waste according to Republic?

☐ Yes ☒ No

If yes, which one?

Approved by Special Waste Review Committee?

☐ Yes ☐ No ☒ Not Applicable

Precautions, Conditions or Limitations on Approval

This waste must be able to pass a paint filter test prior to shipment and disposal.
Free liquids are not permitted for landfill disposal.

Special Waste Analyst Signature:

Date: 7/30/2014

Name (Printed): Suzanne Glass

III. Facility Decision:

☐ Approved ☐ Rejected

Precautions, Conditions or Limitations on Approval

By signing below, the General Manager or Designee agrees that a fully executed Special Waste Service Agreement is on file for this profile and that the special waste file is complete.

General Manager or Designee: _____

Name (Printed): _____

Date: 7/30/2014



Requested Disposal Facility: 3026 East Carolina Regional LF NC

Waste Profile #

30261412720

Saveable fill-in form. Restricted printing until all required (yellow) fields are completed.

I. Generator Information

Sales Rep #: 512

Generator Name: USCG Base Elizabeth City			
Generator Site Address: 1664 Weeksville Road			
City: Elizabeth City	County: Pasquotank	State: North Carolina	Zip: 27909
State ID/Reg No: n/a	State Approval/Waste Code: n/a (if applicable)		NAICS #: n/a
Generator Mailing Address (if different): <input type="checkbox"/> 1664 Weeksville Road			
City: Elizabeth City	County:	State: North Carolina	Zip: 27909
Generator Contact Name: Peter R. Carroll, CDR			Email: Peter.R.Carroll@uscg.mil
Phone Number: (216) 902-6204	Ext:	Fax Number: (216) 902-6277	

II. Billing Information

Bill To: TBD		Contact Name:	
Billing Address:		Email:	
City:	State:	Zip:	Phone:

III. Waste Stream Information

Name of Waste: Dredged Material	
Process Generating Waste: Dredged sediment material at the MWR Boat Ramp at the USCG Base Elizabeth City. The sediment material is dredged to prevent shoaling. The material is generated through natural deposition along the Pasquotank River. The material is not generated through an industrial or manufacturing process.	
Type of Waste:	<input type="checkbox"/> INDUSTRIAL PROCESS WASTE <input checked="" type="checkbox"/> POLLUTION CONTROL WASTE
Physical State:	<input checked="" type="checkbox"/> SOLID <input type="checkbox"/> SEMI-SOLID <input type="checkbox"/> POWDER <input type="checkbox"/> LIQUID
Method of Shipment:	<input checked="" type="checkbox"/> BULK <input type="checkbox"/> DRUM <input type="checkbox"/> BAGGED <input type="checkbox"/> OTHER:
Estimated Annual Volume:	755 Cubic Yards
Frequency:	<input checked="" type="checkbox"/> ONE TIME <input type="checkbox"/> ONGOING
Disposal Consideration:	<input checked="" type="checkbox"/> LANDFILL <input type="checkbox"/> SOLIDIFICATION <input type="checkbox"/> BIOREMEDIATION

IV. Representative Sample Certification

☐ NO SAMPLE TAKEN

Is the representative sample collected to prepare this profile and laboratory analysis, collected in accordance with U.S. EPA 40 CFR 261.20(c) guidelines or equivalent rules?	<input checked="" type="checkbox"/> YES or <input type="checkbox"/> NO
Type of Sample: <input checked="" type="checkbox"/> COMPOSITE SAMPLE <input type="checkbox"/> GRAB SAMPLE	
Sample Date: 05/15/2014	
Sample ID Numbers: MWR-1 MWR-2	



Waste Profile #

30261412720

V. Physical Characteristics of Waste

Characteristic Components		% by Weight (range)			
1. Metals		<1% (0.237-3.44 ppm)			
2. Water Content		18 - 19%			
3. Sediment		80 - 81%			
4.					
5.					
Color	Odor (describe)	Does Waste Contain Free Liquids?	% Solids	pH:	Flash Point
Tan and gray	None	<input type="checkbox"/> YES or <input checked="" type="checkbox"/> NO	80-81	7.7-7.8	>140 °F

Attach Laboratory Analytical Report (and/or Material Safety Data Sheet) Including Chain of Custody and Required Parameters Provided for this Profile

Does this waste or generating process contain regulated concentrations of the following Pesticides and/or Herbicides: Chlordane, Endrin, Heptachlor (and its epoxides), Lindane, Methoxychlor, Toxaphene, 2,4-D, or 2,4,5-TP Silvex as defined in 40 CFR 261.33?	<input type="checkbox"/> Yes or <input checked="" type="checkbox"/> No
Does this waste contain reactive sulfides (greater than 500 ppm) or reactive cyanide (greater than 250 ppm)[reference 40 CFR 261.23(a)(5)]?	<input type="checkbox"/> Yes or <input checked="" type="checkbox"/> No
Does this waste contain regulated concentrations of Polychlorinated Biphenyls (PCBs) as defined in 40 CFR Part 761?	<input type="checkbox"/> Yes or <input checked="" type="checkbox"/> No
Does this waste contain concentrations of listed hazardous wastes defined in 40 CFR 261.31, 261.32, 261.33, including RCRA F-Listed Solvents?	<input type="checkbox"/> Yes or <input checked="" type="checkbox"/> No
Does this waste exhibit a Hazardous Characteristic as defined by Federal and/or State regulations?	<input type="checkbox"/> Yes or <input checked="" type="checkbox"/> No
Does this waste contain regulated concentrations of 2,3,7,8-Tetrachlorodibenzodioxin (2,3,7,8-TCDD), or any other dioxin as defined in 40 CFR 261.31?	<input type="checkbox"/> Yes or <input checked="" type="checkbox"/> No
Is this a regulated Radioactive Waste as defined by Federal and/or State regulations?	<input type="checkbox"/> Yes or <input checked="" type="checkbox"/> No
Is this a regulated Medical or Infectious Waste as defined by Federal and/or State regulations?	<input type="checkbox"/> Yes or <input checked="" type="checkbox"/> No
Is this waste a reactive or heat generating waste?	<input type="checkbox"/> Yes or <input checked="" type="checkbox"/> No
Does the waste contain sulfur or sulfur by-products?	<input type="checkbox"/> Yes or <input checked="" type="checkbox"/> No
Is this waste generated at a Federal Superfund Clean Up Site?	<input type="checkbox"/> Yes or <input checked="" type="checkbox"/> No
Is this waste from a TSD facility, TSD like facility or consolidator?	<input type="checkbox"/> Yes or <input checked="" type="checkbox"/> No

VI. Certification

I hereby certify that to the best of my knowledge and belief, the information contained herein is a true, complete and accurate description of the waste material being offered for disposal and all known or suspected hazards have been disclosed. All Analytical Results/Material Safety Data Sheets submitted are truthful and complete and are representative of the waste.

I further certify that by utilizing this profile, neither myself nor any other employee of the company will deliver for disposal or attempt to deliver for disposal any waste which is classified as toxic waste, hazardous waste or infectious waste, or any other waste material this facility is prohibited from accepting by law. I shall immediately give written notice of any change or condition pertaining to the waste not provided herein. Our company hereby agrees to fully indemnify this disposal facility against any damages resulting from this certification being inaccurate or untrue.

I further certify that the company has not altered the form or content of this profile sheet as provided by Republic Services Inc.

Peter R. Carroll, CDR

U. S. Coast Guard Civil Engineering Unit

Authorized Representative Name And Title (Type or Print)

Company Name

Authorized Representative Signature

Date

About AECOM

AECOM is a premier, fully integrated professional and technical services firm positioned to design, build, finance and operate infrastructure assets around the world for public- and private-sector clients. With nearly 100,000 employees — including architects, engineers, designers, planners, scientists and management and construction services professionals — serving clients in over 150 countries around the world, AECOM is ranked as the #1 engineering design firm by revenue in Engineering News-Record magazine's annual industry rankings. The company is a leader in all of the key markets that it serves, including transportation, facilities, environmental, energy, oil and gas, water, high-rise buildings and government. AECOM provides a blend of global reach, local knowledge, innovation and technical excellence in delivering customized and creative solutions that meet the needs of clients' projects. A Fortune 500 firm, AECOM companies, including URS Corporation and Hunt Construction Group, have annual revenue of approximately \$19 billion.

More information on AECOM and its services can be found at www.aecom.com.